

7/10/2023

Brad Ryan
City Manager
Municipality of Skagway
700 Spring St.
Skagway, AK 99840

RE: Skagway Ore Peninsula Redevelopment Descoping Pricing

Please find attached Pacific Pile & Marine's (PPM) proposal for the Skagway Ore Peninsula Redevelopment De-Scoping Pricing based on drawings received 07/05/2023 and discussions thereafter.

Revised Validity

- **The milestone dates included in addendum 16 are amended as follows:**
- **Please see revised proposal dated 07/10/2023. This pricing assumes notice of award and notice to proceed (NTP) will be provided by MOS on or before 07/20/2023.**
- **PPM recognizes that award of the Additional MSP Scope of Works is subject to negotiations between MOS and the Yukon Province and that award and NTP will not be provided on 07/20/2023. However, to extend the validity of the MSP bid item, the MSP must be awarded before 09/01/2023 as the bid validity on all material prices expire at the end of August. After the end of August, inflationary pressure on steel and pipe piles may result in PPM having to amend the MSP pricing.**

Documents Included:

Appendix A: Pricing for Concept De-Scoping Plan – 07/05/2023

Appendix B: Updated P6 Schedule Based on De-Scoping Plan (to follow)

Appendix C: PPM Contract Clarifications and Exceptions

Appendix D: Concept Drawings provided by KPFF

Appendix E: PPM Demolition Scope Drawing

Appendix F: PPM Pile Table Markup

Appendix G: Haskell Temp Fuel Line Quote and Exclusions

CLARIFICATIONS

Demolition – See Appendix E

PPM has amended demolition pricing based on the descoping drawing provided by KPFF on 07/05/2023 and has provided an additional marked up drawing to clarify the scope.

Demolition Inclusions:

Season 1

- *Existing Ore Loader*
 - PPM will abate, cut, lift-off/remove, demolish, and dispose of the existing Ore Loader.
 - PPM will include the appropriate BMPs involved when disposing of a structure with this level of contamination.
- *Existing Mooring Dolphin A (Dolphin 3)*
 - PPM to remove existing dolphin cap and gangway to be modified.
- *Existing Dolphin B (Dolphin 4) and Existing Dolphin C (Dolphin 5)*
 - PPM to remove and dispose existing fenders and fender piles at both locations and protect-in-place existing spin-fin piles and existing caps per drawing D1.01
- *Existing Mooring Dolphins D, E, and F*
 - PPM to remove and dispose 3 existing mooring dolphins including piles, fenders, and caps at each location per drawing D1.01.
 - Existing spin-fin piles (11ea) will be cut-off at mudline by divers.
- *Existing Dolphins G and H*
 - PPM to remove and dispose 2 existing dolphins including piles, fenders, and caps at each location per drawing D1.02.
 - Existing spin-fin piles (6ea) will be cut-off at mudline by divers.
- *Existing Concrete Dock*
 - PPM will demolish and dispose of the existing concrete dock as shown on drawing D1.02.
 - Existing spin-fin piles (41ea) will be cut-off at mudline by divers.
- *Existing South In-Fill and Existing Covered Walkway*
 - PPM and our subcontractors will demolish and dispose the existing South In-Fill and Covered Walkway timber dock as shown on drawing D1.03 and D1.04.
 - Creosote timber associated with this dock will be disposed at an approved upland facility.
- *Existing Dolphin #3*
 - PPM will demolish and dispose Existing Dolphin #3 including piles, fender, and cap per drawing D1.03.
- *Existing Dolphin #4*
 - PPM will demolish and dispose Existing Dolphin #4 including piles, fender, and cap per drawing D1.04.
- *Existing Ore Loader Supporting Concrete Structure*
 - PPM will demolish and dispose of the existing Ore Loader Supporting Concrete Structure on the offshore end of the Ore Loader Structure including the cap and supporting piles per drawing D1.04.
 - PPM will install temporary falsework to support the cap during demolition.

- PPM will sawcut the concrete cap into manageable sizes to be lifted by PPM's Derrick Barge the Pacific Lifter.
- *Existing Gangways*
 - PPM will lift off and salvage existing gangways where shown and called out on drawings D1.01 through D1.04.

Season 2

- *Existing Dolphin 5*
 - PPM will demolish and dispose Existing Dolphin #4 including piles, fender, and cap per drawing D1.04.
 - Existing Dolphin 5 is mis-numbered on drawing G8.05 and the Concept De-Scoping Plan – 07/05/2023 drawing provided by KPFF. This dolphin will be removed and disposed of in Season 2 and will be considered as part of the MSP Additive item.
- *Existing Timber Middle In-Fill and Timber Ore Dock*
 - PPM and our subcontractors will partially demolish and dispose of the existing Middle In-Fill and Ore Dock as shown on drawing provided by Ed Debroeck 07/07/2023, also shown on D1.04 and D1.05.
 - It is assumed that approximately 50% of each structure will be demolished.
 - All creosote timber piles will be disposed of at the appropriate approved upland facility.
- *Existing Dolphin #6 and Existing Barge Dolphin #1*
 - Existing dolphins above will be demolished and disposed including piles, fenders, and caps.

Demolition Exclusions:

- **Existing Ore Conveyor No. 2 Supporting Concrete platform and piling demolition is Excluded.**
- **Existing Fuel Header and Fuel Lines running out to the dock demolition is Excluded.**
- **Remainder of Existing Middle In-Fill not mentioned above (approximately 50%) is excluded.**
- **Remainder of Existing Ore Dock and Walkways not mentioned above (approximately 50%) is excluded.**
- **Existing Barge Dolphins # 2 and #3 is excluded.**
- **Existing Dolphin #7 is excluded.**
- **Existing Tug Dolphin 2 and its associated catwalk is excluded.**
- **Any other demolition not specifically stated in the inclusions is excluded.**

Piling and Pile Driving

- It is PPM's understanding that the updated scope of works provided on 07/05/2023 does not require PPM to purchase new 48", 42", 36", or 24" piles and associated plug plates for the base bid.
- PPM includes the purchase of 2 each 30" Diameter by 0.75" Wall-Thickness piles (Pile #100 and #101) associated with Catwalk Support #1.
- **PPM acknowledges that concurrent pile driving with the use of vibratory hammers, impact hammers, or any combination thereof is expected to not be allowed by the permits and therefore PPM has included that in this price.**

- This pricing excludes installation of the offshore dolphins D1 and D2. Piles for these dolphins (48" and 42") have been supplied by MOS and it has been assumed PPM will transport them up to Skagway and offload them onto shore in their supplied sizes. PPM excludes any splicing on piles #1 through #12.
- PPM has included a PDA on Dolphin 3 due to the deletion of Dolphins 1 and 2. There are 7 PDAs total in the base bid.
- Pile supported structures to be built and included in the **Base Bid** price are as followed:
 - Dolphin 3 (Pile #13 through Pile #17)
 - Cruise Dock Float Guide Piles (Pile #18 through Pile #31)
 - Cruise Dock Trestle Piles (Pile #32 through Pile #43)
 - Dolphin 6 (Pile #48 through Pile #57)
 - Dolphin 7 (Pile #58 through Pile #67)
 - Catwalk Supports (Pile #100 through Pile #103)
 - Dolphin 4 Fender Piles (Pile #106 through Pile #108)
 - Dolphin 5 Fender Piles (Pile #109 through Pile #111)
- Pile supported structures to be built and included in the **Marine Services Platform (MSP)** price are as followed:
 - Marine Services Platform (Pile #M001 through Pile #M100)
 - All piles and plug plates to be procured by PPM
 - MSP Fender Piles (Pile #M101 through Pile #M106)
 - All piles and plug plates to be procured by PPM
 - MSP Dolphin S1 (Pile #M107 through Pile #M114)
 - Pile #'s M107, M108, M109 to be procured by PPM
 - Pile #'s M111, M112, M113, M114 previously procured by MOS in Skagway
 - MSP Dolphin N1 (Pile #M115 through Pile #M122)
 - Pile #'s M115, M116, M117, M120, M121 to be procured by PPM
 - Pile #'s M118, M119, M122 previously procured by MOS in Skagway
 - MSP Dolphin N1 now to be installed in Season 2 per instructions from KPFF and MOS. N1 was to be installed in Season 1 for the original bid submission.
- **Pile supported structures specifically excluded from this pricing include:**
 - Dolphins 1, 2, and 8
 - Fuel Header, Bridge, Platform, and On-Shore Fuel Line Supports
 - Catwalk Support #3
 - RORO Trestle
 - Any pile supported structure not explicitly stated in the inclusions

Electrical

- Electrical has been excluded from both the base bid and the MSP as part of this bid.

Temp Fuel Header

- PPM has included pricing for Haskell Corporation to install the temporary fuel header along the Broadway dock as shown in the plans.
- Haskell Corporation will remove and salvage the temporary fuel header along the Broadway Dock and cap it at or near the 90 degree turn at the base of the dock at the completion of the cruise ship pier and following reinstatement of the existing fuel line.



Pacific Pile & Marine, LP
700 South Riverside Drive
Seattle, WA 98108

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F 206 774-5958
License # PACIFPM922J3

- Haskell will reinstate the existing fuel line at the completion of the Cruise Ship Pier prior to Milestone #1 on April 21, 2024.
- Please see attached Appendix G with Haskell's quote, concept drawings, and exclusions previously agreed upon between KPFF and Haskell.

Should you have any questions, please feel free to contact me direct at (206) 892-8472.

Thank you,

A handwritten signature in black ink, appearing to be 'CW', followed by a long horizontal line extending to the right.

Chris Willis
Executive Vice President
Pacific Pile & Marine, LP

Appendix A: Pricing for
Concept De-Scoping Plan –
07/05/2023

REQUEST FOR BIDS – BID FORM

(Contractor’s name below)

Pacific Pile & Marine, LP _____ agrees to provide all labor, equipment, transportation, materials and mobilization and demobilization to and from the work site to complete the Project as described in the Request for Bids dated and in any bid addenda for the total lump sum of:

\$ 62,887,152.92
(Numeric Dollar Amount)

\$ Sixty Two Million Eight Hundred Eighty Seven Thousand One Hundred Fifty Two dollars & Ninety Two Cents
(Written Dollar Amount)

Base Bid					
Item No	Description of Work	Qty.	Unit	Unit Price	Total
1	Mobilization, Demobilization & General Conditions	1	LS	12,248,683.93	12,248,683.93
2	Marine TESC	1	LS	307,842.05	307,842.05
3	Upland TESC	1	LS	100,288.53	100,288.53
4	Survey	1	LS	529,762.36	529,762.36
5	Cruise Dock Float Assembly and Installation	1	LS	297,592.66	297,592.66
6	Ore Loader Demolition	1	LS	1,841,487.22	1,841,487.22
7	Marine Demolition	1	LS	4,035,212.95	4,035,212.95
7A	Spin Fin Pile Cutoff	58	EA	7,237.27	419,761.66
8	Upland Demolition	4	LS		
9A	Contaminated Soils Excavation & Handling	2385	CY		
9B	Contaminated Soils Stabilization	2385	CY		
9C	Stabilized Soils Haul & Disposal	2385	CY		
10	Upland Excavation & Soils Management	4	LS		
11	Upland Concrete Paving	4	LS		
12	Upland Utilities	4	LS		
13	Temporary Fuel Piping	1	LS	2,047,375.23	2,047,375.23
14	Marine Utilities & Fuel Piping	4	LS		
15	Upland Appurtenances & Handrail	4	LS		
16	Upland Railroad Track Replacement	4	LS		
17	Cast in Place Concrete - Marine	1	LS	281,687.58	281,687.58
18	Precast Concrete	1	LS	590,328.04	590,328.04
19	Structural Steel	1	LS	7,447,893.38	7,447,893.38
20	Furnish & Deliver Piling	1	LS	110,371.28	110,371.28
21	Pile Driving & Splicing	1	LS	4,003,757.55	4,003,757.55
22	Pile PDA Testing	1	LS	143,599.63	143,599.63
23	Anode Procurement & Installation	1	LS	202,866.96	202,866.96

24	Electrical	1	LS		
25	Contractor Delivery of MOS Procured Items	1	LS	680,170.89	680,170.89
Subtotal TOTAL (BASE BID)					\$ 35,288,684.90
Total Base Bid					\$ 35,288,684.90

Force Account Items

Item No	Description of Work	Qty.	Unit	Unit Price	Total
26	Hazardous Soils Haul & Disposal	17	Ton	847.98	14,415.66
26A	Underwater Debris – Removal & Disposal	10	Ton	2,719.06	27,190.60
27	Pile Driving – Overdrive Allowance 24” Dia. Pile	50	LF	23.35	1,167.50
28	Pile Driving – Overdrive Allowance 36” Dia. Pile	240	LF	23.41	5,618.40
29	Pile Driving – Overdrive Allowance 42” Dia. Pile	110	LF	50.24	5,526.40
30	Pile Driving – Overdrive Allowance 48” Dia. Pile	60	LF	50.31	3,018.60
31	Pile Driving – Restrikes	50	EA	11,058.36	552,918.00
32	Pile Driving - Obstruction Removal/ Redrive	3	EA	72,449.49	217,348.47
33	Marine Mammal Delays (1 Hour Block)	75	EA	6,290.48	471,786.00
34	Weather Related Delays (1/2 Shift)	15	EA	29,248.65	438,729.75
Force Account - TOTAL					\$ 1,737,719.38
Base Bid + Force Account - TOTAL					\$ 37,026,404.28

Marine Services Platform - Add Alt

Item No	Description of Work	Qty.	Unit	Unit Price	Total
35	MSP Mobilization, Demobilization & General Conditions	1	LS	5,094,070.31	5,094,070.31
36	MSP Marine TESC	1	LS	17,378.89	17,378.89
37	MSP Upland TESC	1	LS		
38	MSP Marine Demolition	1	LS	913,075.17	913,075.17
39	MSP Marine Surveys	1	LS	292,977.65	292,977.65
40	MSP Upland Excavation	159	CY	31.40	4,992.60
41	MSP Upland EX: Imported Backfill	57	CY	65.18	3,715.26
42	MSP Upland EX: Base Course, Grading D1	71	CY	92.43	6,562.53
43	MSP Upland Appurtenances	1	LS		
44	MSP Upland Railroad Track Replacement	1	LS		
45	MSP Structural Steel	1	LS	7,335,370.19	7,335,370.19
46	MSP Precast Concrete	1	LS	3,014,444.12	3,014,444.12
47	MSP Cast in Place Concrete	1	LS	841,043.90	841,043.90
48	MSP Pile Procurement & Delivery	1	LS	4,997,117.34	4,997,117.34
49	MSP Pile Driving & Splicing	1	LS	2,358,176.48	2,358,176.48
50	MSP Pile PDA Testing	1	LS	71,656.29	71,656.29
51	MSP Anode Procurement & Installation	1	LS	346,119.25	346,119.25
52	MSP Electrical	1	LS		
53	MSP Marine Utilities & Stormwater	1	LS		

54	MSP Fendering	1	LS	154,059.66	154,059.66
ADD ALT MSP – TOTAL					\$ 25,450,759.64

MSP Force Account Items - Add Alt

Item No	Description of Work	Qty.	Unit	Unit Price	Total
56	MSP Pile Driving - Overdrive Allowance 24" Dia.	3502 50	LF	23.35	1,167.50
57	MSP Pile Driving - Overdrive Allowance 36" Dia.	244 50	LF	23.35	1,167.50
58	MSP Pile Driving – Restrikes	10	EA	7,023.08	70,230.80
59	MSP Pile Driving - Obstruction Removal/ Redrive	1	EA	33,710.75	33,710.75
60	MSP Marine Mammal Delays (1 Hour Block)	50	EA	3,266.69	163,334.50
61	MSP Weather Related Delays (1/2 Shift)	15	EA	9,358.73	140,380.95
MSP FORCE ACCOUNT - TOTAL					409,992.00
ADD ALT MSP + MSP Force Account – TOTAL					\$ 25,860,751.64

Base Bid + ADD ALT MSP \$ 60,739,441.54
Base Force Account + MSP Force Account \$ 2,147,711.38
TOTAL (Basis of Award) \$ 62,887,152.92

The Basis of Award will be based on the lowest responsive bidder who presents the lowest cost to complete the scopes of work contained in the Base Bid, the Marine Services Platform (Additive Alternate), and the Force Accounts for both the Base Bid and the Add Alt.

Force Account quantities are only an assumed approximate quantity, actual quantities could vary to be zero or greater than shown.

**As of the bid date soil sample testing results have not been completed. An addendum will be issued with updated quantities for the estimated quantity of contaminated soil onsite.*

ACKNOWLEDGEMENT OF ADDENDUMS

The bidder acknowledges receipt of addendums to the solicitation (give number and date of each)

<u>Addendum #</u>	<u>Date</u>	Addendum #	Date
1	5/15/23	9	6/9/23
2	5/18/23	10	6/12/23
3	5/26/23	11	6/13/23
4	6/2/23	12	6/14/23
5	6/5/23	13	6/15/23
6	6/6/23	14	6/16/23
7	6/7/23	15	6/21/23
8	6/8/23	16	6/21/23
		17	6/29/23

**Appendix B: Updated P6
Schedule Based on
De-Scoping Plan (to follow)**

Appendix C: PPM Contract Clarifications and Exceptions



Pacific Pile & Marine, LP
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License # PACIFPM922J3

July 3rd, 2023

Municipality of Skagway
PO Box 415
Skagway, Alaska 99840

Attn: Municipality of Skagway
KPPF Consulting Engineers

RE: Ore Peninsula Redevelopment Project – Contract
Clarifications and Exceptions

Dear Municipality of Skagway,

We have reviewed the Municipality of Skagway (“MOS”) Sample Construction Services Agreement and Insurance Requirements for the Ore Terminal 2023 Large Ship Mooring project. Our bid and the associated bonds are contingent upon MOS and Pacific Pile & Marine, LP (“PPM”) reaching mutually agreeable contractual terms addressing the specific issues outlined below. Our proposed amendments are as follows:

1. Contractor may invoice and MOS shall pay 5% of the awarded contract value upon award and receipt of the signed Contract and after MOS has received Contractor’s Payment and Performance Bonds as advanced payment to fund mobilization.
2. For all bid items that containing materials and equipment not incorporated in the Work but delivered and suitably stored in Seattle or another secured location prior to delivery to sight, Contractor may request and MOS shall pay for such materials. The Application for Payment shall be accompanied by manufacturer or supplier’s invoice or other documentation and evidence that the Materials and Equipment are covered by appropriate property insurance and other arrangements to protect the MOS’s interest therein, all of which will be satisfactory to the Engineer. Access to material storage facilities will be provided to Quality Control Staff, Owner or Owner’s agents for inspection or verification of materials.
3. Contractor cannot accept \$40,000/day liquidated damages and as such, Contract Article VII. A. 2). is deleted in its entirety and replaced to read as follows:
 - A. 2). For Project Milestone 1 as defined in Specification Section 01 14 00 Work Restrictions and Permits the CONTRACTOR will pay the MOS up to \$5,000 per day as liquidated damages if the project is not completed in accordance with the Proposal specifications. If the MOS determines that the project is defective and that repairs must be made to meet the Proposal specifications, the CONTRACTOR will pay the MOS up to \$5,000 per day for each day that the project fails to meet the approval of the MOS, up to the time that the MOS agrees that the project has been completed in accordance with the Proposal specifications. If there are any certifications or permits necessary for acceptance of the project, the project shall not be determined complete until the CONTRACTOR has secured all such certifications or permits and liquidated damages shall continue to accrue.
4. MOS shall, upon request from Contractor, provide reasonable evidence that financial arrangements have been made and secured to fulfill Owners’ obligations under the Contract.
5. Article 4.3 Differing Site Conditions is Amended to read as follows:
 - A. Contractor has had no ability to reasonably inspect subsurface site conditions and has only visually reviewed the work site. “Differing Site Condition” shall mean: (1)

subsurface or latent physical conditions at the site which materially differ from those indicated in the Contract Documents; (2) unknown physical conditions at the site, of an unusual nature, which differ materially from those ordinarily encountered and generally recognized as inherent in the work of the character provided for in the Contract Documents; and/or (3) discovery of any Unknown Hazardous Materials.

- B. In the event CONTRACOR encounters a Differing Site Condition, Contractor shall promptly, and before the conditions are disturbed, give written notice to the Engineer.
 - C. The Engineer shall investigate the site conditions promptly after receiving the notice. If the conditions do meet the definition of Differing Site Condition and cause an increase or decrease in the Contractor's cost of, or the time required for, performing any part of the work under this contract, whether or not changed as a result of the conditions, an equitable adjustment shall be made under this clause and a Change Order will issue.
 - D. No request by the Contractor for an equitable adjustment to the contract under this clause shall be allowed, unless the Contractor has given the written notice required.
 - E. If the Engineer does not determine a Differing Site Conditions exists, the Engineer will provide written justification to CONTRACTOR within 7 days. If Contractor disagrees with the Project Manager's decision, Contractor may file a Claim pursuant to Articles 11 and 12.
6. Hazardous Materials:
Contractor shall not be responsible for any Hazardous Environmental Condition encountered at the site (including all subsurface materials) which are not identified in the Contract Documents to be within the scope of the Work. Under no circumstance shall Contractor be identified as the generator of such hazardous materials. Contractor shall only be responsible for hazardous materials brought to the Site by Contractor and its Subcontractors, Suppliers, or anyone else for whom Contractor is responsible.
7. Design Changes:
To the extent any changes by Owner and/or Owner's engineer to the plans and specifications increase or decrease Contractor's time and/or cost of performance, Owner shall issue a change order incorporating such changes to and include an equitable adjustment to the contract time and price.
8. Article 11.5 A. is deleted in its entirety and is replaced with the following:
11.5 A. Payroll costs and other compensation of the CONTRACTOR's officers, executives, principals of partnerships and sole proprietorships, general managers, engineers, architects, estimators, lawyers, auditors, accountants, purchasing and contracting agents, expeditors, timekeepers, clerks, superintendents and non-working foremen, and similar administrative personnel not working directly on the Work. These costs shall be considered administrative costs covered by the CONTRACTOR's Fee. For purposes of clarity, CONTRACTOR'S engineers, purchasing and contracting agents, clerks, superintendents, and administrative personnel working on the Project shall be considered "Cost of Work."
9. As a result of MOS's requirement that General Conditions be included in Mobilization and Demobilization costs, notwithstanding Article 1.05 of Section 01 22 00, Contractor shall invoice and MOS shall pay Mobilization and Demobilization costs pursuant to the Schedule of Values.
10. The force account rates for piling overdrive in both the base and the MSP additive items do not include the cost of a pile splice if required. Pile splices cannot be priced on a linear foot basis which is the basis of measurement for the force account items. Contractor and Client to agree a force account item for pile splices after award.
11. The force account rates for obstruction removal and redrive do not include the provision and cost of providing drilling equipment to drill through an unremovable obstruction. If drilling equipment is required the cost of mobilizing and drilling will be paid on a cost plus basis and take into account increase in overhead and delay time as a result.

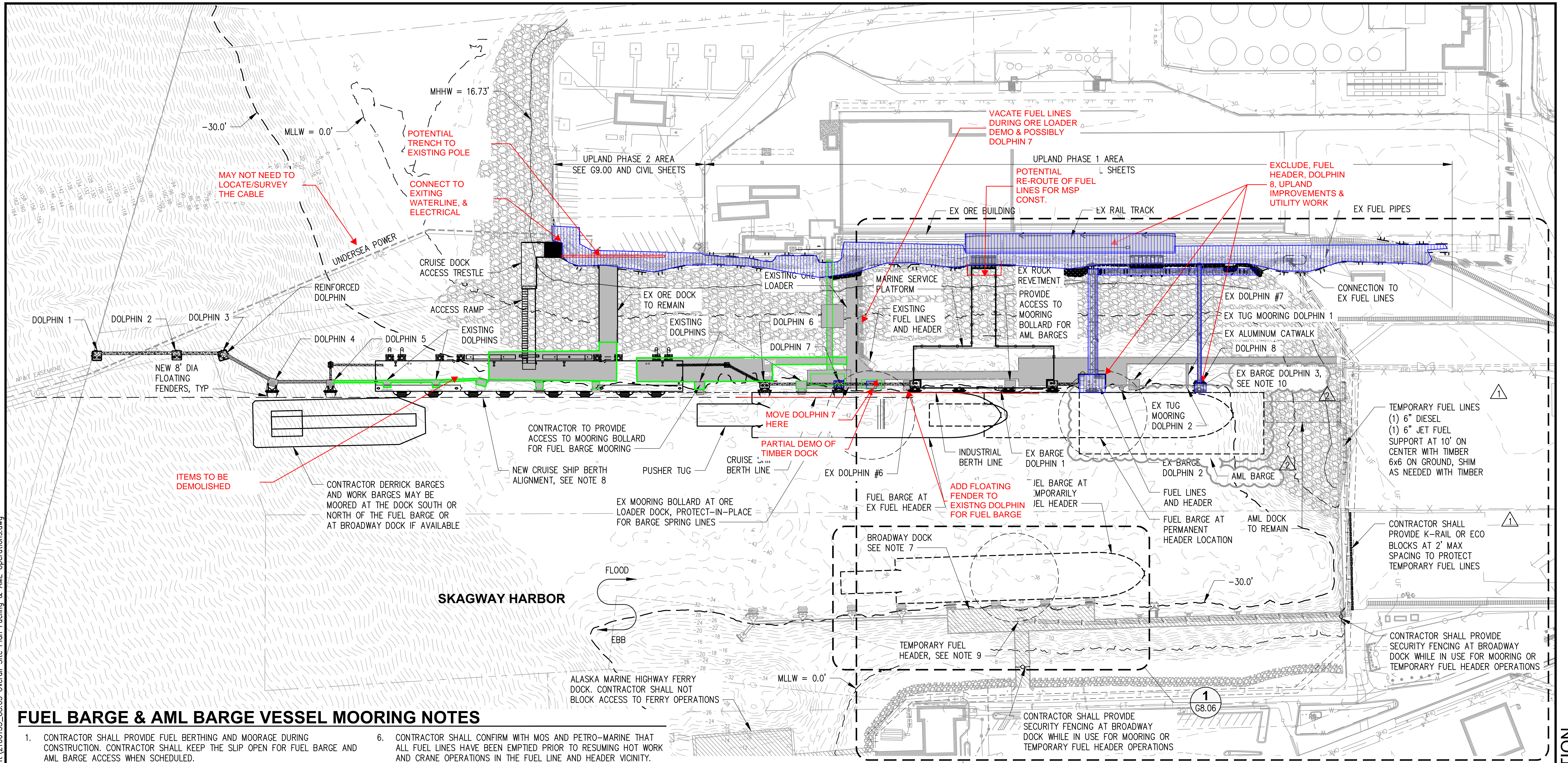
Sincerely,

Chris Willis
Executive Vice President Pacific
Pile & Marine, LP

Appendix D: Concept Drawings

Concept De-Scoping Plan - 07/05/2023

Plotted: Jun 02, 2023 - 11:24am d:\u M:\2021\2100135 Skagway Ore Peninsula Multi-Use Dock\Drawings\Current\2100135_08.05 Overall Site Plan Fueling & AML Operations.dwg



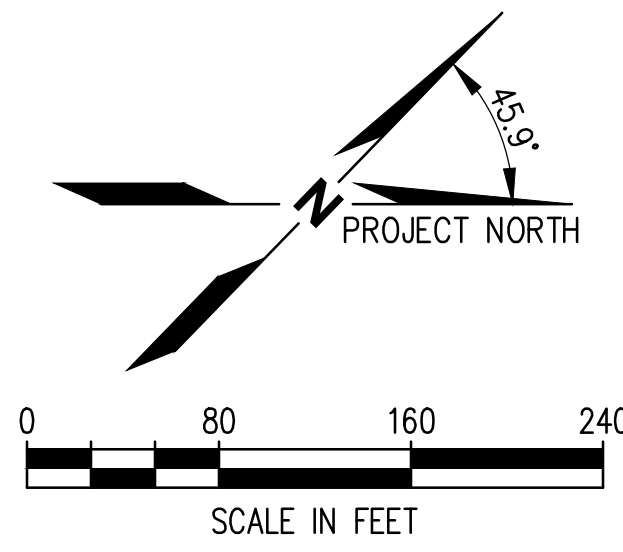
FUEL BARGE & AML BARGE VESSEL MOORING NOTES

- CONTRACTOR SHALL PROVIDE FUEL BERTHING AND MOORAGE DURING CONSTRUCTION. CONTRACTOR SHALL KEEP THE SLIP OPEN FOR FUEL BARGE AND AML BARGE ACCESS WHEN SCHEDULED.
- FUEL BARGE IS GENERALLY EXPECTED EVERY 20 DAYS. FUEL TRANSFER DURATION IS TYPICALLY 36-48 HOURS. AML BARGES TYPICALLY LOAD WEEKLY SUNDAY-MONDAY AND CAN VARY. CONTRACTOR TO COORDINATE WITH MOS ON SCHEDULING. CONTRACTOR SHALL KEEP THE ORE BASIN OPEN FOR AML BARGE TRAFFIC WHEN SCHEDULED.
- NO HOT WORK WILL BE ALLOWED WITHIN 300' OF THE FUEL BARGE WHILE IN PORT. NO CRANE WORK WITHIN RANGE OF THE BARGE WILL BE PERMITTED.
- NEW AND EXISTING STRUCTURES SHOWN IN THIS VIEW, CONTRACTOR SHALL DETERMINE PHASING OF PROJECT TO ALLOW FOR FUEL BARGE OPERATIONS AND AML BARGE OPERATIONS.
- CONTRACTOR SHALL SUBMIT FUEL BARGE MOORING AND BERTHING PLANS 3 WEEKS PRIOR TO FUEL BARGE ARRIVAL.
- CONTRACTOR SHALL CONFIRM WITH MOS AND PETRO-MARINE THAT ALL FUEL LINES HAVE BEEN EMPTIED PRIOR TO RESUMING HOT WORK AND CRANE OPERATIONS IN THE FUEL LINE AND HEADER VICINITY. SEE G SHEETS FOR PETRO-MARINE CONTACT INFORMATION.
- CONTRACTOR MAY MOOR VESSELS AND BARGES AT BROADWAY DOCK WITH FLOATING FENDERS REMOVED BY CONTRACTOR TO PREVENT DAMAGE. FLOATING FENDERS SHALL BE REMOVED FOR FUEL BARGE OPERATIONS.
- NOTE EXISTING BERTH LINE AND CURRENT BERTH LINE DIFFER IN LOCATION.
- TEMPORARY FUEL HEADER. CONTRACTOR DESIGNED, CONTRACTOR TO PROVIDE CONNECTIONS FOR FUEL LINES. SEE SHEET G8.06.
- CONTRACTOR MAY TEMPORARILY RELOCATE THE PLUMB PILE FROM BARGE DOLPHIN 3 TO ACCOMMODATE AML BARGE BERTHING DURING CONSTRUCTION OF DOLPHIN 8.

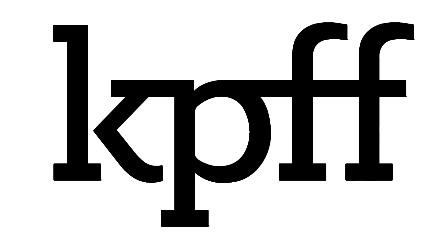
OVERALL SITE PLAN
SCALE: 1" = 80'

LEGEND

- EXISTING STRUCTURES TO BE REMOVED, SEE DEMOLITION SHEETS
- EXISTING FUEL LINES
- FUEL LINES
- SECURITY FENCE



CONCEPT DE-SCOPING PLAN - 07/05/2023



1601 5th Avenue, Suite 1300
Seattle, Washington 98101
(206) 382-0600 Fax (206) 382-0500



NO.	DATE	BY	REVISION
1	05/18/23	JLF	ADDENDUM 1
2	06/02/23	EJD	ADDENDUM 3



ORE PENINSULA REDEVELOPMENT
SKAGWAY, ALASKA

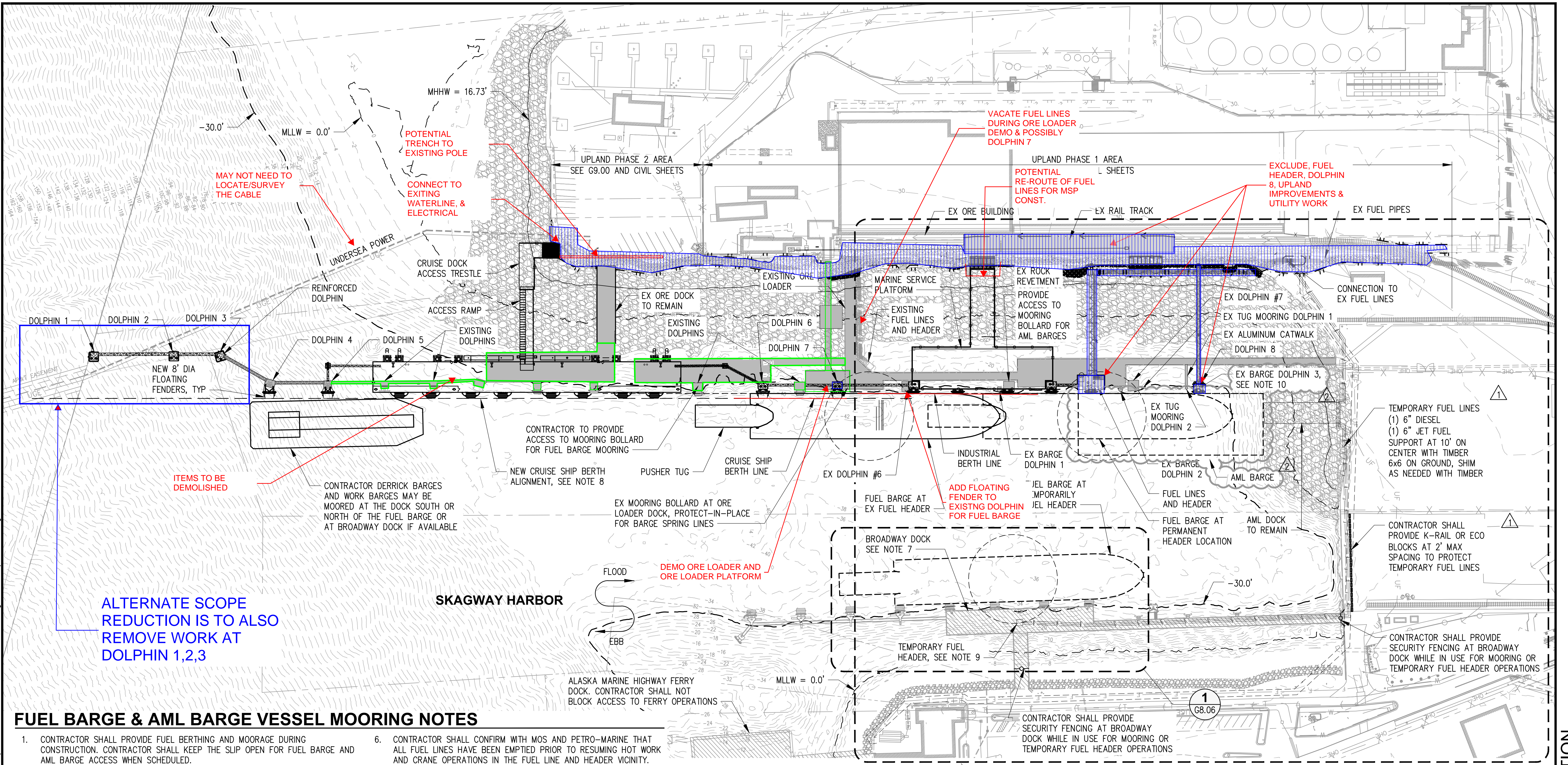
OVERALL SITE PLAN - CONSTRUCTION
FUELING AND AML BARGE OPERATIONS

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: RR	DATE: 05/04/2023
DRAWING NO.	G8.05
SHEET NO.	14 OF 374

BID SET - NOT FOR CONSTRUCTION

Concept De-Scoping
Plan REV1 -
07/06/2023

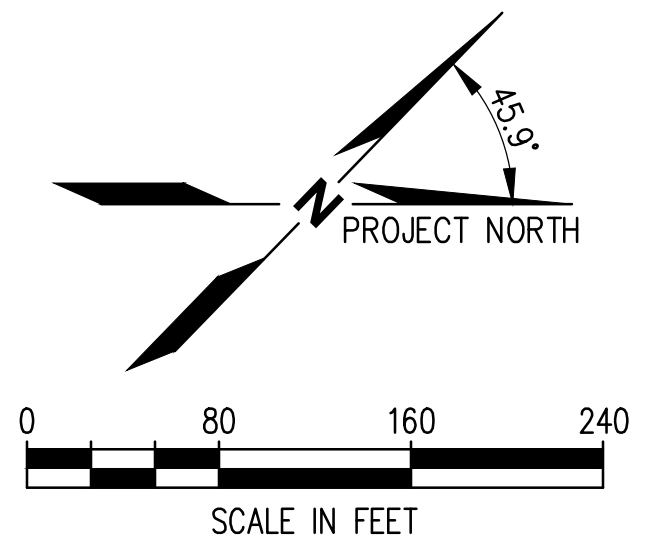
Plotted: Jun 02, 2023 - 11:24am d:\u0001\2100135 Skagway Ore Peninsula Multi-Use Dock\Drawings\Current\2100135_08.05 Overall Site Plan Fueling & AML Operations.dwg
 M:\2021\2100135 Skagway Ore Peninsula Multi-Use Dock\Drawings\Current\2100135_08.05 Overall Site Plan Fueling & AML Operations.dwg



OVERALL SITE PLAN
SCALE: 1" = 80'

LEGEND

- EXISTING STRUCTURES TO BE REMOVED, SEE DEMOLITION SHEETS
- EXISTING FUEL LINES
- FUEL LINES
- SECURITY FENCE



NO.	DATE	BY	REVISION
1	05/18/23	JLF	ADDENDUM 1
2	06/02/23	EJD	ADDENDUM 3



ORE PENINSULA REDEVELOPMENT
SKAGWAY, ALASKA

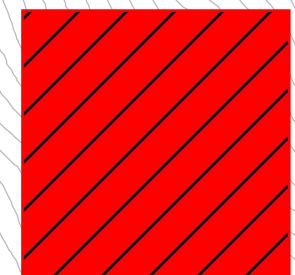
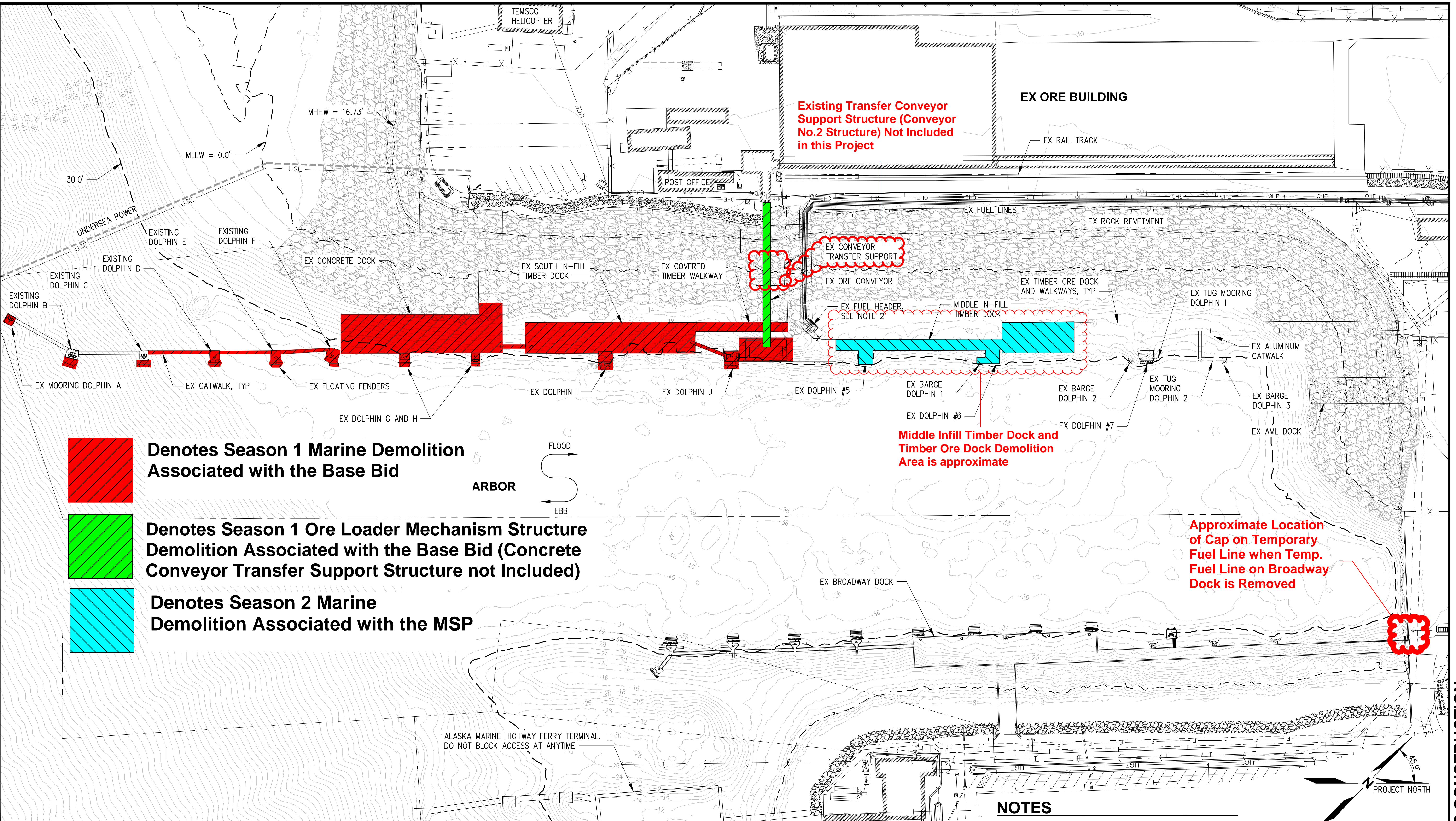
OVERALL SITE PLAN - CONSTRUCTION
FUELING AND AML BARGE OPERATIONS

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: RR	DATE: 05/04/2023
DRAWING NO.	G8.05
SHEET NO.	14 OF 374

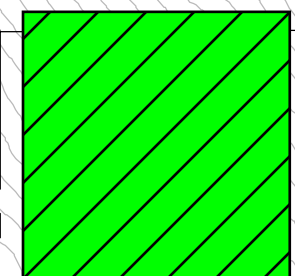
BID SET - NOT FOR CONSTRUCTION

Appendix E: PPM Demolition Scope Drawing

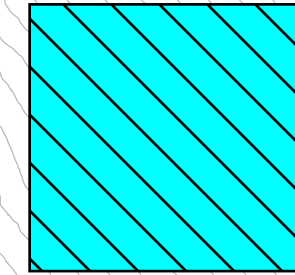
Plotted: May 08, 2023 - 11:13am dw Layout: G7.00
M:\2021\2100135 Skagway Ore Peninsula Multi-use Dock Drawings\Current\2100135_G7.00 Existing Conditions Site Plan.dwg



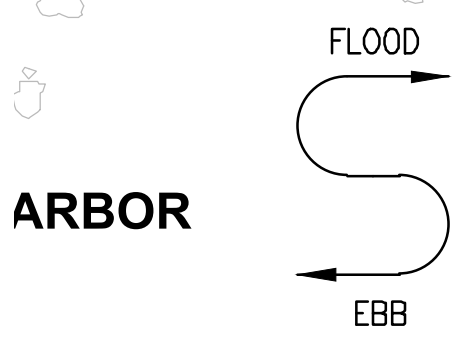
Denotes Season 1 Marine Demolition Associated with the Base Bid



Denotes Season 1 Ore Loader Mechanism Structure Demolition Associated with the Base Bid (Concrete Conveyor Transfer Support Structure not Included)



Denotes Season 2 Marine Demolition Associated with the MSP



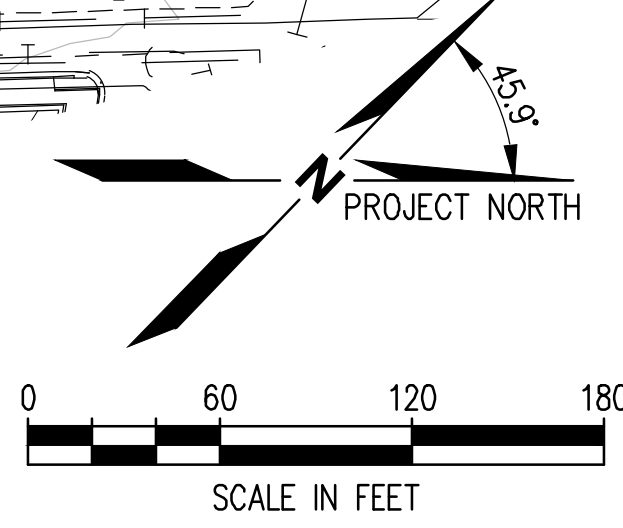
ARBOR

ALASKA MARINE HIGHWAY FERRY TERMINAL. DO NOT BLOCK ACCESS AT ANYTIME

EXISTING CONDITIONS
SCALE: 1" = 60'

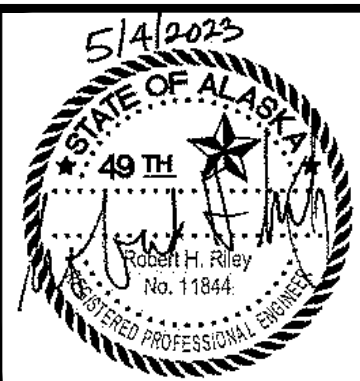
NOTES

- EXISTING DOLPHIN NUMBERING IS BASED ON 1984 RECORD DRAWING AND 1992 ORE DOCK EXPANSION RECORD DRAWING.
- FUEL HEADER AND LINES ARE KEPT FULL. CONTRACTOR SHALL PROTECT-IN-PLACE PRIOR TO DEMOLITION.



kpff

1601 5th Avenue, Suite 1300
Seattle, Washington 98101
(206) 382-0600 Fax (206) 382-0500



NO.	DATE	BY	REVISION



**ORE PENINSULA REDEVELOPMENT
SKAGWAY, ALASKA**

EXISTING CONDITIONS SITE

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: RR	DATE: 05/04/2023
DRAWING NO.	G7.00
SHEET NO.	8 OF 374

BID SET - NOT FOR CONSTRUCTION

Appendix F: PPM Pile Table Markup

6

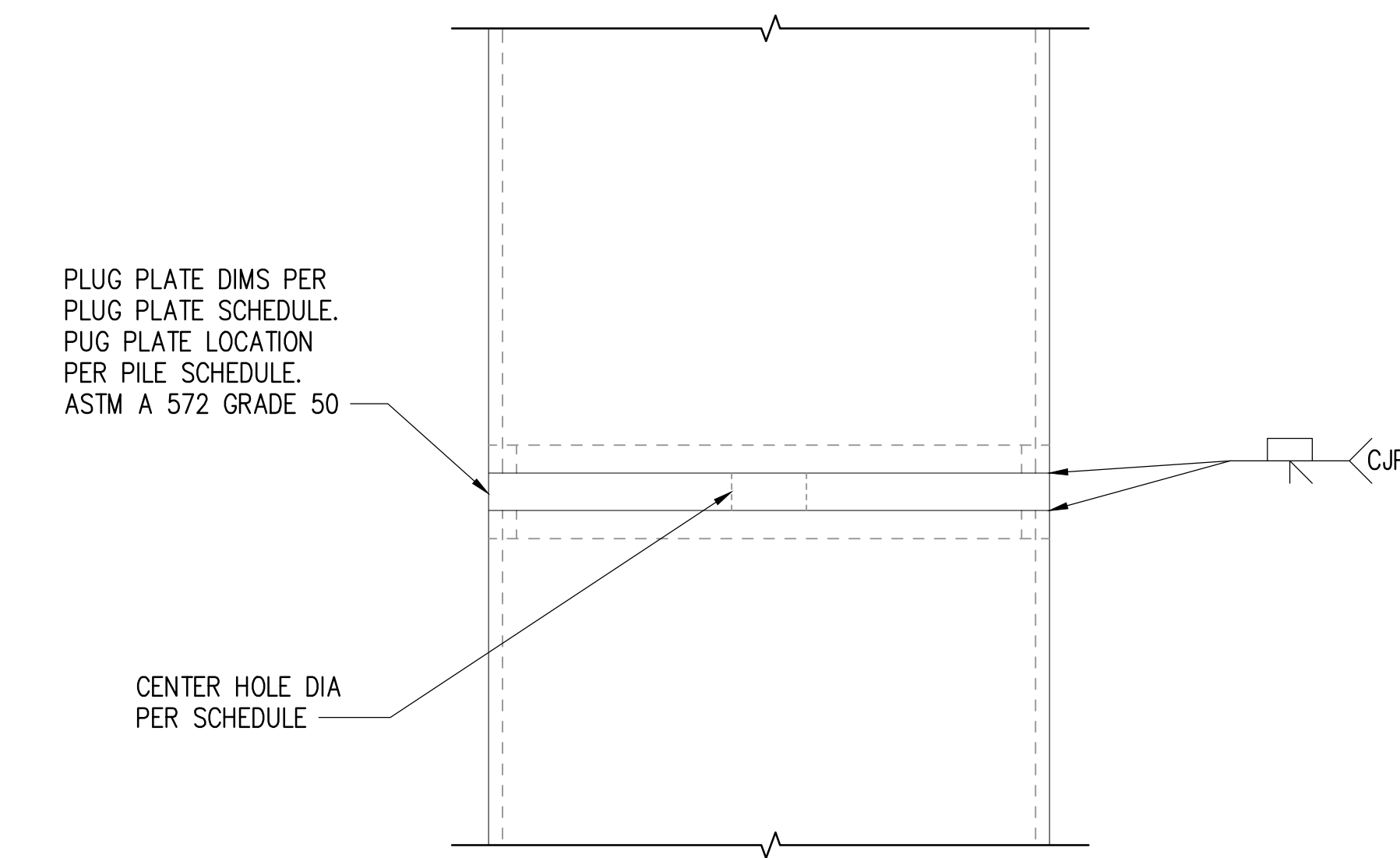
PILE SCHEDULE													
PILE	OD	WALL THK	f _y	PILE CUT-OFF ELEV	ESTIMATED MUDDLINE ELEV	DESIGN PILE TIP ELEV	COATED LENGTH	BATTER	REQ CAP TENSION	REQ CAP COMPR	PROVIDED OVERDRIVE LENGTH	PLUG PLATE DISTANCE FROM PILE TIP	NOTES
DOLPHIN 1													
1	48.00	1.00	50.00	25.50	-152.00	-200.00	208.00	12:1	135	22	20.00	N/A	SUPPLIED BY MOS IN SEATTLE
2	48.00	1.00	50.00	25.50	-137.00	-214.00	202.00	3:1	338	875	20.00	75.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
3	48.00	1.00	50.00	25.50	-141.00	-208.00	206.00	3:1	-	801	20.00	65.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
4	48.00	1.00	50.00	25.50	-154.00	-236.00	220.00	3:1	-	1232	20.00	81.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
5	48.00	1.00	50.00	25.50	-138.00	-240.00	226.00	3:1	-	1164	20.00	79.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
6	48.00	1.00	50.00	25.50	-157.00	-258.00	223.00	3:1	680	428	20.00	N/A	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
DOLPHIN 2													
7	42.00	1.00	50.00	25.50	-121.00	-163.00	177.00	N/A	79	62	20.00	N/A	SUPPLIED BY MOS IN SEATTLE
8	42.00	1.00	50.00	25.50	-105.00	-210.00	168.00	3:1	914	891	20.00	N/A	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
9	42.00	1.00	50.00	25.50	-113.00	-171.00	177.00	3:1	35	210	20.00	55.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
10	42.00	1.00	50.00	25.50	-127.00	-227.00	163.00	3:1	-	1559	20.00	99.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
11	42.00	1.00	50.00	25.50	-137.00	-192.00	202.00	3:1	27	672	20.00	53.00	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
12	42.00	1.00	50.00	25.50	-131.00	-238.00	195.00	3:1	925	485	20.00	N/A	SUPPLIED BY MOS IN SEATTLE. SEE NOTE 9
DOLPHIN 3													
13	42.00	1.00	50.00	25.50	-88.00	-164.00	150.00	3:1	341	151	20.00	N/A	SUPPLIED BY MOS IN SEATTLE
14	42.00	1.00	50.00	25.50	-89.00	-166.00	152.00	3:1	356	833	20.00	75.00	SUPPLIED BY MOS IN SEATTLE
15	42.00	1.00	50.00	25.50	-102.00	-179.00	165.00	3:1	-	782	20.00	75.00	SUPPLIED BY MOS IN SEATTLE
16	42.00	1.00	50.00	25.50	-115.00	-191.00	178.00	3:1	-	694	20.00	75.00	SUPPLIED BY MOS IN SEATTLE
17	42.00	1.00	50.00	25.50	-115.00	-191.00	178.00	3:1	235	590	20.00	75.00	SUPPLIED BY MOS IN SEATTLE
CRUISE DOCK FLOAT GUIDE PILES													
18	36.00	1.00	50.00	41.50	-36.00	-109.00	100.00	4:1	40	720	10.00	70.00	SUPPLIED BY MOS IN SEATTLE
19	36.00	1.00	50.00	41.50	-36.00	-109.00	100.00	4:1	40	720	10.00	70.00	SUPPLIED BY MOS IN SEATTLE
20	36.00	1.00	50.00	41.50	-17.00	-95.00	81.00	4:1	40	720	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
21	36.00	1.00	50.00	41.50	-17.00	-95.00	81.00	4:1	40	720	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
22	36.00	1.00	50.00	42.50	-40.00	-138.00	103.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY. PDA TEST REQUIRED
23	36.00	1.00	50.00	42.50	-40.00	-138.00	103.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
24	36.00	1.00	50.00	42.50	-35.00	-133.00	98.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
25	36.00	1.00	50.00	42.50	-32.00	-130.00	95.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
26	36.00	1.00	50.00	42.50	-24.00	-122.00	87.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
27	36.00	1.00	50.00	42.50	-24.00	-122.00	87.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
28	36.00	1.00	50.00	42.50	-24.00	-122.00	87.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
29	36.00	1.00	50.00	42.50	-24.00	-123.00	87.00	N/A	710	760	10.00	N/A	SUPPLIED BY MOS IN SEATTLE. PDA TEST REQUIRED
30	24.00	0.50	50.00	44.00	-7.00	-89.00	71.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
31	24.00	0.50	50.00	44.00	-7.00	-89.00	71.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
CRUISE DOCK TRESTLE PILES													
32	24.00	0.75	50.00	25.83	13.00	-77.00	33.00	N/A	-	290	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
33	24.00	0.75	50.00	25.83	18.00	-72.00	28.00	N/A	-	351	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
34	24.00	0.75	50.00	25.83	24.00	-67.00	23.00	N/A	-	267	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
35	24.00	0.75	50.00	25.83	12.00	-78.00	34.00	N/A	-	350	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
36	24.00	0.75	50.00	25.83	17.00	-74.00	30.00	N/A	-	414	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
37	24.00	0.75	50.00	25.83	22.00	-68.00	24.00	N/A	-	274	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
38	24.00	0.75	50.00	25.83	11.00	-79.00	35.00	N/A	-	349	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
39	24.00	0.75	50.00	25.83	15.00	-75.00	31.00	N/A	-	365	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
40	24.00	0.75	50.00	23.52	10.00	-81.00	34.00	N/A	-	348	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
41	24.00	0.75	50.00	23.52	12.00	-78.00	32.00	N/A	-	358	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY. PDA TEST REQUIRED
42	24.00	0.75	50.00	23.52	8.00	-73.00	36.00	N/A	-	642	10.00	75.00	SUPPLIED BY MOS IN SEATTLE
43	24.00	0.75	50.00	23.52	9.00	-72.00	36.00	N/A	-	634	10.00	75.00	SUPPLIED BY MOS IN SEATTLE. PDA TEST REQUIRED
DOLPHIN 6													
48	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	790	350	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
49	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	728	28	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
50	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	790	350	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
51	36.00	1.00	50.00	24.00	-41.00	-132.00	99.00	3:1	612	826	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
52	36.00	1.00	50.00	24.00	-41.00	-132.00	99.00	3:1	612	826	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
53	36.00	1.00	50.00	24.00	-41.00	-116.00	97.00	6:1	412	686	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
54	36.00	1.00	50.00	24.00	-41.00	-116.00	97.00	6:1	412	686	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
55	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
56	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
57	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE

PILE SCHEDULE													
PILE	OD	WALL THK	f _y	PILE CUT-OFF ELEV	ESTIMATED MUDDLINE ELEV	DESIGN PILE TIP ELEV	COATED LENGTH	BATTER	REQ CAP TENSION	REQ CAP COMPR	PROVIDED OVERDRIVE LENGTH	PLUG PLATE DISTANCE FROM PILE TIP	NOTES
DOLPHIN 4													
58	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	790	350	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
59	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	728	28	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
60	36.00	1.00	50.00	26.00	-30.00	-132.00	89.00	3:1	790	350	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
61	36.00	1.00	50.00	24.00	-41.00	-132.00	99.00	3:1	612	826	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
62	36.00	1.00	50.00	24.00	-41.00	-132.00	99.00	3:1	612	826	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
63	36.00	1.00	50.00	24.00	-41.00	-116.00	97.00	6:1	412	686	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
64	36.00	1.00	50.00	24.00	-41.00	-116.00	97.00	6:1	412	686	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. SEE NOTE 9
65	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
66	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
67	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SEATTLE
DOLPHIN 5													
68	36.00	1.00	50.00	26.00	-30.00	-87.00	89.00	3:1	176	676	20.00	55.00	SEE NOTE 9
69	36.00	1.00	50.00	26.00	-30.00	-87.00	89.00	3:1	176	676	20.00	55.00	SEE NOTE 9
70	36.00	1.00	50.00	24.00	-41.00	-123.00	99.00	3:1	524	508	20.00	80.00	SEE NOTE 9
71	36.00	1.00	50.00	24.00	-41.00	-122.00	96.00	N/A	428	128	20.00	75.00	SEE NOTE 9
72	36.00	1.00	50.00	24.00	-41.00	-123.00	99.00	3:1	524	508	20.00	80.00	SEE NOTE 9
73	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
74	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
75	24.00	0.75	50.00	29.00	-45.00	-101.00	94.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
FUEL HEADER, BRIDGE, PLATFORM, AND ON-SHORE FUEL LINE SUPPORTS													
76	24.00	0.75	50.00	29.00	-28.00	-98.00	77.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
77	24.00	0.75	50.00	29.00	-28.00	-98.00	77.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
78	24.00	0.75	50.00	29.00	-28.00	-98.00	77.00	N/A	-	-	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
79	36.00	1.00	50.00	26.00	-26.00	-125.00	85.00	3:1	208	702	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY. PDA TEST REQUIRED
80	36.00	1.00	50.00	26.00	-26.00	-125.00	85.00	3:1	218	698	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
81	36.00	1.00	50.00	26.00	-28.00	-129.00	87.00	3:1	512	394	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
82	36.00	1.00	50.00	26.00	-28.00	-129.00	84.00	N/A	462	348	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
83	36.00	1.00	50.00	26.00	-28.00	-100.00	87.00	3:1	368	482	20.00	N/A	SUPPLIED BY MOS IN SKAGWAY
84	36.00	0.75	50.00	26.94	-26.00	-101.00	73.00	N/A	114	268	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
85	36.00	0.75	50.00	26.94	-26.00	-101.00	73.00	N/A	118	272	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
86	36.00	0.75	50.00	26.94	-6.00	-83.00	53.00	N/A	82	338	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
87	36.00	0.75	50.00	26.94	-6.00	-83.00	53.00	N/A	82	338	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
88	24.00	0.75	50.00	26.98	22.00	-48.00	25.00	N/A	28	130	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
89	24.00	0.75	50.00	26.98	22.00	-48.00	25.00	N/A	100	144	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
90	24.00	0.75	50.00	26.98	22.00	-48.00	25.00	N/A	-	244	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
91	24.00	0.75	50.00	26.98	22.00	-48.00	25.00	N/A	44	166	10.00	N/A	SUPPLIED BY MOS IN SKAGWAY
92	24.00	0.75	50.00	26.40	22.00	-48.00	25.00						

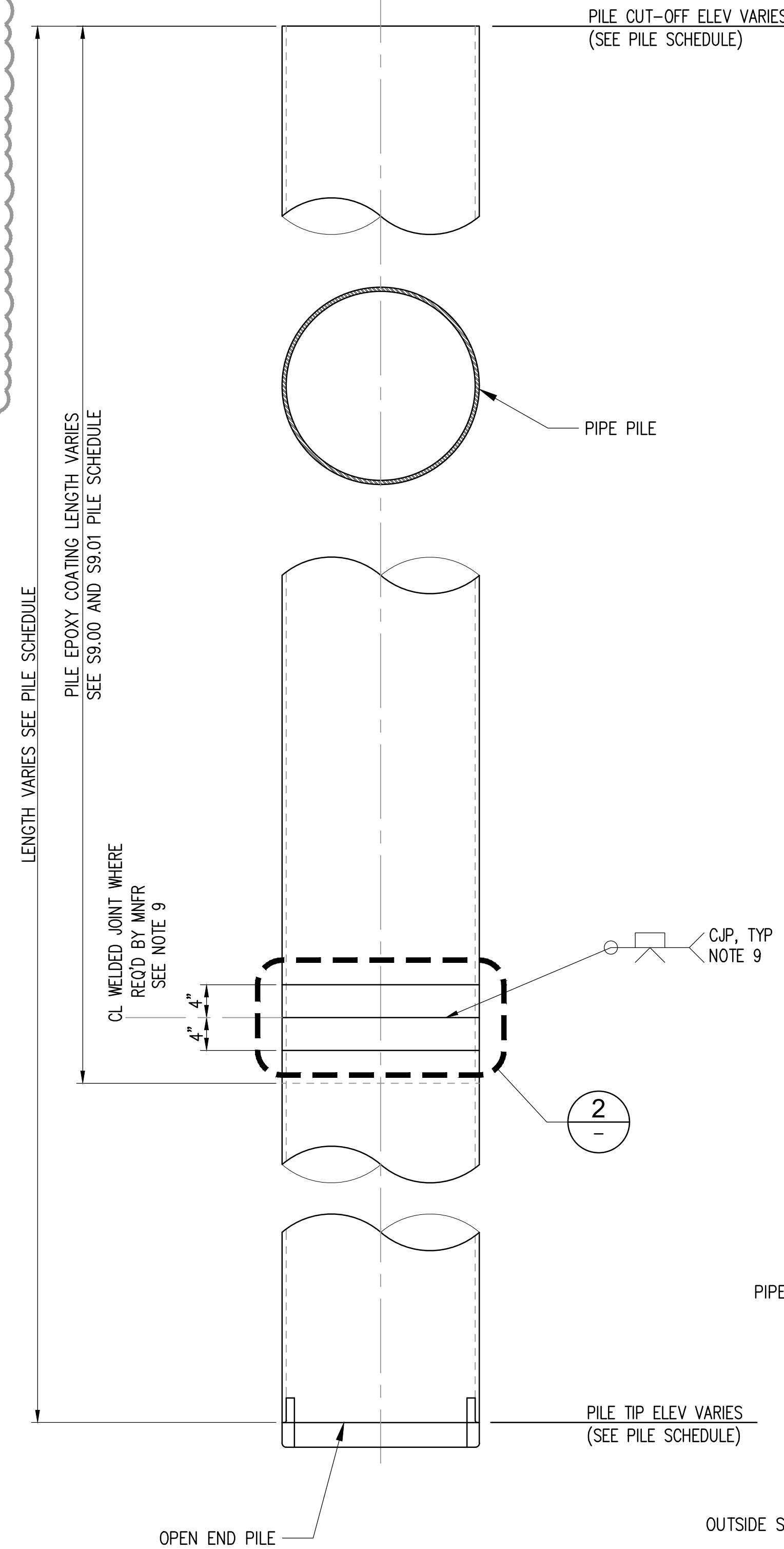
PILE SCHEDULE															
PILE	GRID	GRID	WALL			PILE CUT-OFF ELEV	ESTIMATED MUDLINE ELEV	DESIGN PILE TIP ELEV	COATED LENGTH	BATTER	REQ CAP TENSION	REQ CAP COMPR-SSION	PROVIDED OVERDRIVE LENGTH	PLUG PLATE DISTANCE FROM PILE TIP	NOTES
			OD [in]	THK [in]	f _y [ksi]										
RORO TRESTLE															
R001	A	1	24.00	0.75	50.00	26.67	26.00	-44.00	21.00	N/A	104	728	10.00	65.00	
R002	A	2	24.00	0.75	50.00	26.67	26.00	-44.00	21.00	N/A	104	728	10.00	65.00	
R003	A	3	24.00	0.75	50.00	26.67	26.00	-44.00	21.00	N/A	104	728	10.00	65.00	
R004	A	4	24.00	0.75	50.00	26.67	26.00	-44.00	21.00	N/A	104	728	10.00	65.00	
R005	B	1	24.00	0.75	50.00	27.58	27.00	-63.00	21.00	N/A	104	728	10.00	85.00	
R006	B	2	24.00	0.75	50.00	27.58	27.00	-63.00	21.00	N/A	104	728	10.00	85.00	
R007	B	3	24.00	0.75	50.00	27.58	27.00	-63.00	21.00	N/A	104	728	10.00	85.00	
R008	B	4	24.00	0.75	50.00	27.58	27.00	-63.00	21.00	N/A	104	728	10.00	85.00	
R009	C	1	24.00	0.75	50.00	27.48	22.00	-68.00	26.00	N/A	56	664	10.00	85.00	
R010	C	2	24.00	0.75	50.00	27.48	22.00	-68.00	26.00	N/A	56	664	10.00	85.00	
R011	C	3	24.00	0.75	50.00	27.48	22.00	-68.00	26.00	N/A	56	664	10.00	85.00	
R012	C	4	24.00	0.75	50.00	27.48	22.00	-68.00	26.00	N/A	56	664	10.00	85.00	

Piles to be installed in new scope

~~Piles not included in new scope~~



3 PLUG PLATE DETAIL
SCALE: NTS



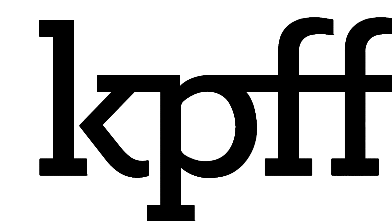
1 STEEL PILE DETAIL
SCALE: 1" = 1'-0"

2 ALTERNATE CJP TUBULAR BUTT JOINT
SCALE: 1:1

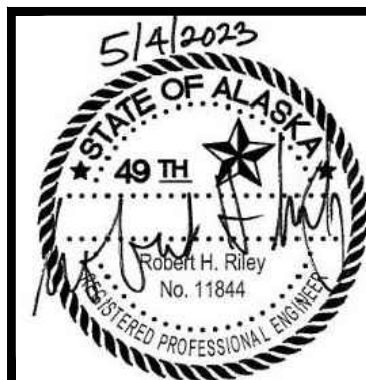
NOTES

- CONTRACTOR SHALL INCLUDE PILE DRIVING TO DESIGN TIP PLUS 10 FEET IN BASE BID.
- IT IS ESTIMATED THAT EMBEDDED LENGTH OF PILES INTO THE SOILS COULD REQUIRE ADDITIONAL EMBEDDED LENGTH TO ACHIEVE REQUIRED CAPACITIES. PILES THAT ARE REQUIRED TO BE DRIVEN FURTHER THAN 10 FEET BEYOND THE ESTIMATED PILE TIP AS DIRECTED BY THE ENGINEER SHALL BE PAYABLE PER LINEAR FOOT AS DESCRIBED IN THE PROJECT SPECIFICATIONS.
- MOS SUPPLIED PILE LENGTHS ARE LISTED IN THE SPECIFICATIONS APPENDIX. THE CONTRACTOR SHALL SPLICE PILES TO THE DESIRED LENGTHS FOR DRIVING AND HANDLING. ADDITIONAL PILE SPLICING AS REQUIRED FOR OVER DRIVE ALLOWANCE IS INCIDENTAL TO THE FORCE ACCOUNT PILE DRIVING ITEMS.
- PILES SHALL BE STORED, SHIPPED AND DELIVERED BY SUPPLIER. AT ALL TIMES PILES SHALL BE STORED WITH CRIBBING TO PREVENT TO DAMAGE COATINGS OR ROLLING OF THE PILES.
- MOS SHALL INSPECT COATINGS AFTER DELIVERY. IF COATING DAMAGE IS FOUND, CONTRACTOR SHALL REPAIR AT NO ADDITIONAL COST TO MOS.
- DEFINED REQUIRED TENSION AND COMPRESSION CAPACITIES ARE ULTIMATE VALUES AND ARE NOT FACTORED.
- SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- WELDING SHALL CONFORM TO "STRUCTURAL WELDING CODE - STEEL", AWS D1.1.
- HOLD BACK OR REMOVE EPOXY COATING FROM CENTERLINE WELDED JOINT ON INSIDE AND OUTSIDE OF PIPE BEFORE WELDING. AFTER JOINT IS WELDED, APPLY EPOXY COATINGS PER SPECIFICATIONS FOR REPAIR OF PILE GALVANIZED COATING.
- MANUFACTURER MAY USE THE ALTERNATE CJP TUBULAR BUTT JOINT. SEE DETAIL 2 ON THIS SHEET. ALL CJP TUBULAR BUTT JOINTS SHALL HAVE A WPS QUALIFIED IN ACCORDANCE WITH AWS D1.1 SECTION 4.
- NOTE THAT PILES SUPPLIED BY MOS IN SKAGWAY MAY BE LONGER THAN THE REQUIRED LENGTH. SEE PRE-PROCUREMENT PILE LIST IN SPECIFICATION APPENDIX.

Plotted: May 25, 2023 - 5:52pm d:\u00012100135 Skagway Ore Peninsula Multi-Use Dock Drawings\Current\2100135_S9.01 General Pile Schedule.dwg



1601 5th Avenue, Suite 1300
Seattle, Washington 98101
(206) 382-0600 Fax (206) 382-0500



NO.	DATE	BY	REVISION
2	05/25/23	KPT	ADDENDUM 2



**ORE PENINSULA REDEVELOPMENT
SKAGWAY, ALASKA**

GENERAL PILE SCHEDULE

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: RR	DATE: 05/04/2023
DRAWING NO.	S9.01
SHEET NO.	252 OF 374

BID SET - NOT FOR CONSTRUCTION

PILE SCHEDULE														
PILE ID	GRD	OD	WALL THK	f _r	PILE CUT-OFF ELEV	ESTIMATED MUDDLINE ELEV	DESIGN PILE TIP ELEV	COATED LENGTH	REQ CAP TENSION	REQ CAP COMPRESS	PROVIDED OVERDRIVE LENGTH	PLUG PLATE DISTANCE FROM PILE TIP	NOTES	
MARINE SERVICE PLATFORM														
M001	A'	7	24.00	0.75	50.00	25.73	25.00	-40.00	21.00	N/A	-	540	10.00	60.00
M002	A'	8	24.00	0.75	50.00	25.73	25.00	-40.00	21.00	N/A	-	540	10.00	60.00
M003	A'	9	24.00	0.75	50.00	25.73	25.00	-40.00	21.00	N/A	-	540	10.00	60.00
M004	A	7	24.00	0.75	50.00	25.66	26.00	-85.00	20.00	N/A	-	847	10.00	105.00
M005	A	8	24.00	0.75	50.00	25.66	26.00	-85.00	20.00	N/A	-	847	10.00	105.00
M006	A	9	24.00	0.75	50.00	25.66	26.00	-85.00	20.00	N/A	-	847	10.00	105.00
M007	B	7	24.00	0.75	50.00	25.50	20.00	-81.00	26.00	N/A	-	751	10.00	95.00
M008	B	8	24.00	0.75	50.00	25.50	20.00	-81.00	26.00	N/A	-	751	10.00	95.00
M009	B	9	24.00	0.75	50.00	25.50	20.00	-81.00	26.00	N/A	-	751	10.00	95.00
M010	C	7	24.00	0.75	50.00	25.34	14.00	-86.00	32.00	N/A	-	751	10.00	95.00
M011	C	8	24.00	0.75	50.00	25.34	14.00	-86.00	32.00	N/A	-	751	10.00	95.00
M012	C	9	24.00	0.75	50.00	25.34	14.00	-86.00	32.00	N/A	-	751	10.00	95.00
M013	D	7	24.00	0.75	50.00	25.18	8.00	-92.00	37.00	N/A	-	761	10.00	95.00
M014	D	8	24.00	0.75	50.00	25.18	8.00	-92.00	37.00	N/A	-	761	10.00	95.00
M015	D	9	24.00	0.75	50.00	25.18	8.00	-92.00	37.00	N/A	-	761	10.00	95.00
M016	E	7	24.00	0.75	50.00	25.02	3.00	-101.00	42.00	N/A	-	752	10.00	99.00
M017	E	8	24.00	0.75	50.00	25.02	3.00	-101.00	42.00	N/A	-	752	10.00	99.00
M018	E	9	24.00	0.75	50.00	25.02	3.00	-101.00	42.00	N/A	-	752	10.00	99.00
M019	F	7	24.00	0.75	50.00	24.86	-3.00	-107.00	48.00	N/A	-	760	10.00	99.00
M020	F	8	24.00	0.75	50.00	24.86	-3.00	-107.00	48.00	N/A	-	760	10.00	99.00
M021	F	9	24.00	0.75	50.00	24.86	-3.00	-107.00	48.00	N/A	-	760	10.00	99.00
M022	G	7	24.00	0.75	50.00	24.70	-7.00	-111.00	52.00	N/A	-	768	10.00	99.00
M023	G	8	24.00	0.75	50.00	24.70	-7.00	-111.00	52.00	N/A	-	768	10.00	99.00
M024	G	9	24.00	0.75	50.00	24.70	-7.00	-111.00	52.00	N/A	-	768	10.00	99.00
M025	H	7	24.00	0.75	50.00	24.54	-12.00	-128.00	57.00	N/A	-	740	10.00	111.00
M026	H	8	24.00	0.75	50.00	24.54	-12.00	-128.00	57.00	N/A	-	740	10.00	111.00
M027	H	9	24.00	0.75	50.00	24.54	-12.00	-128.00	57.00	N/A	-	740	10.00	111.00
M028	I	1	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M029	I	2	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M030	I	3	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M031	I	4	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M032	I	5	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M033	I	6	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M034	I	7	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M035	I	8	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M036	I	9	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M037	I	10	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M038	I	11	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M039	I	12	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M040	I	13	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M041	I	14	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M042	I	15	24.00	0.75	50.00	24.42	-15.00	-132.00	60.00	N/A	-	751	10.00	111.00
M043	J	1	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M044	J	2	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M045	J	3	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M046	J	4	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M047	J	5	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M048	J	6	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M049	J	7	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M050	J	8	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M051	J	9	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M052	J	10	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M053	J	11	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M054	J	12	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M055	J	13	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M056	J	14	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M057	J	15	24.00	0.75	50.00	24.58	-19.00	-136.00	64.00	N/A	-	797	10.00	111.00
M058	K	1	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M059	K	2	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M060	K	3	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M061	K	4	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M062	K	5	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M063	K	6	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M064	K	7	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M065	K	8	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M066	K	9	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M067	K	10	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M068	K	11	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M069	K	12	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M070	K	13	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M071	K	14	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00
M072	K	15	24.00	0.75	50.00	24.74	-22.00	-158.00	67.00	N/A	-	772	10.00	131.00

PILE SCHEDULE														
PILE ID	GRD	OD	WALL THK	f _r	PILE CUT-OFF ELEV	ESTIMATED MUDDLINE ELEV	DESIGN PILE TIP ELEV	COATED LENGTH	REQ CAP TENSION	REQ CAP COMPRESS	PROVIDED OVERDRIVE LENGTH	PLUG PLATE DISTANCE FROM PILE TIP	NOTES	
MARINE SERVICE PLATFORM														
M073	L	1	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M074	L	2	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M075	L	3	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M076	L	4	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M077	L	5	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M078	L	6	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M079	L	7	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M080	L	8	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M081	L	9	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M082	L	10	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M083	L	11	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M084	L	12	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M085	L	13	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M086	L	14	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M087	L	15	24.00	0.75	50.00	24.90	-25.00	-161.00	70.00	N/A	-	796	10.00	131.00
M088	M	2	24.00	0.75	50.00	25.06	-31.00	-168.00	77.00	N/A	-	646	10.0	

Appendix G: Haskell Temp Fuel Line Quote and Exclusions



HASKELL
CONSTRUCTORS –SINCE 1890

July 7, 2023

Chris Willis
Chief Estimator
Pacific Pile & Marine
700 S. Riverside Drive
Seattle, WA 98108

SUBJECT: Skagway Ore Peninsula Redevelopment – Temporary Fuel Lines

Mr. Willis:

Haskell Corporation is pleased to present our lump sum (LS) proposal for temporary fuel line scope associated with the Skagway Ore Peninsula Redevelopment Project. Haskell Corporation wishes to thank you for including us in this solicitation, and we look forward to the prospect of working together.

We have reviewed the information you provided in the referenced RFQ. Our proposal is as follows:

SCOPE

Haskell will provide the following services to PPM. All scope below will be supported by a crane and operator supplied by others.

1. Design, supply, and install two temporary fuel lines. The temporary fuel lines will be commissioned by Haskell. Temporary fuel line scope is based on KPFF mark-ups provided on 7/7/2023. KPFF and Petromarine to assist with developing mutually agreeable tie-in plan. Piping at tie-ins will be made safe for hot work by others.
2. Haskell will drain the temporary fuel lines prior to demolition. The temporary fuel lines will be cut into segments less than or equal to sixty feet (60') for removal by others. Demolition of the temporary fuel lines will be limited to piping on the Broadway Dock only. Jersey barriers will be supplied, installed, and left in place.
3. Supply and install temporary hangers at the AML Dock.
4. Supply, install, and remove designed two four (4) foot by eight (8) foot steel plates at the AML dock.

Addenda Received: 16

PRICE

Haskell Corporation proposes to perform the aforementioned scope for the lump sum amount of One Million Seven Hundred Eighty-Five Thousand One Hundred Sixty-Nine Dollars (\$1,785,169) not including Washington State sales Tax WSST. Payment terms are net 30, no retention held. This price is valid for 30 days.

Please add 1.5% to our pricing if a payment and performance bond is required.

SCHEDULE

The scope outlined above has been scheduled on a six (6) day per week ten (10) per day with the option to move to a seven (7) day per week schedule if needed to achieve the required milestone dates. There will be a peak of approximately six (6) craft during the execution of Haskell's scope. Two mobilizations have been assumed for our base scope. Our project execution is based on the following dates and durations:

1. Part of First Mobilization: Contract Award to Haskell on our before 7/18/2023.
2. Part of First Mobilization: Complete temporary fuel pipe installation by 11/1/2023.
3. Part of Second Mobilization: Demolish temporary fuel lines on Broadway Dock in 4/2024.

SUBCONTRACTORS

Temporary Fuel Line Engineering: K Corp.
NDE: TEAM

CLARIFICATIONS

1. This proposal is predicated on negotiation of mutually acceptable terms and conditions that are typical to the industry. No special allowances have been included for such items as delay damages or cost of money. No retainage has been assumed.
2. PPM will provide a builder's risk policy for the project to which Haskell can be named additionally insured. It is assumed the deductibles will be \$50,000 or less.
3. No allowance has been included for dealing with any hazardous materials or differing site conditions that may be encountered while performing this work. This would include hazardous soil remediation, asbestos abatement, or lead abatement costs. In addition, this would include costs to improve ground at location of erection to achieve required bearing pressure as required by the lift plan.
4. No permitting costs or permitting notifications have been included. Haskell's temporary fuel line engineer has included an allowance of 20 hours of permitting support. (01 14 00 1.4).
5. SWPPP installation and maintenance will be by PPM.
6. Snow and ice removal will be by PPM.
7. Temporary site lighting will be by PPM.
8. Dust control is not included.
9. No inspection costs have been included outside of standard inspections for our own work. (00 72 00 13.3).
10. Piping will be hydrotested per specifications and restored. Flushing, pickling, or passivation of new piping is not included.
11. No support for mooring operations of any ships has been included (01 14 00 1.3 G.).
12. The QEP required in specification 01 35 43 will be supplied by PPM.
13. No traffic control or detour materials or labor have been included as required in specification 01 50 50.
14. Water used for hydrotesting will be disposed of at a nearby sanitary sewer connection.

7/7/2023

Page 3 of 3

15. Demolished temporary piping will not be flushed or cleaned. Piping will be capped and stored on site by others for re-installation later. Reinstallation of demolished piping is not included in this proposal.
16. Fuel hoses and containment pans at the temporary fuel header are not included.
17. Integrated shop drawings have not been included.
18. Temporary fuel pipe and fittings will have no coatings. Valves will be coated with the manufacturer's standard factory coating. No touch-up of coatings has been included.
19. Haskell has not included any temporary or permanent fence. PPM will provide the temporary fencing identified in Specification 00 72 00, 6.2.A.2. No modification costs to the existing fence have been included (01 14 00 1.2. A.1).
20. Haskell has included weather protection and enclosures for our scope (when needed).
21. PPM will provide construction waste, chemical toilets, and temporary lighting (00 72 00 6.2.C.1) for use by Haskell.
22. Construction power will be provided by PPM.
23. Surveying and control points for Haskell's scope will be provided by PPM as needed.
24. Parking will be available for Haskell's staff within 2,500 feet of the project site.
25. PPM will provide a crane and operator to lift Haskell's pipe and structural steel materials as needed.
26. Any scope not identified in this letter has not been included.

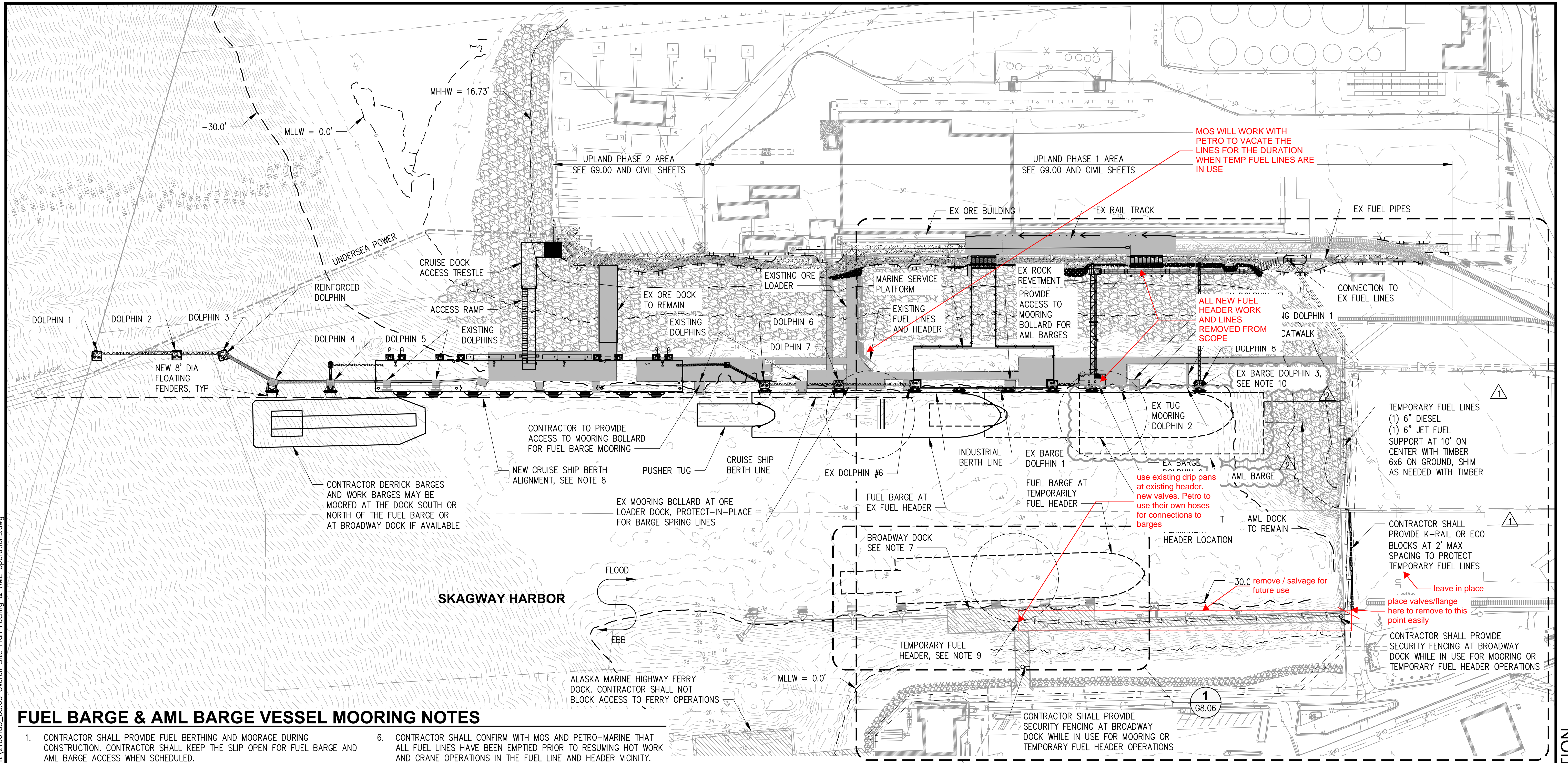
Please direct any questions you may have to the undersigned. I can be reached at (360) 676-7226.

Brett Gunderson
Project Manager
Haskell Corporation

Enclosures

1. KPFF Temporary Fuel Pipe Drawing Markups

Plotted: Jun 02, 2023 - 11:24am d:\u M:\2021\2100135 Skagway Ore Peninsula Multi-Use Dock\Drawings\Current\2100135_08.05 Overall Site Plan Fueling & AML Operations.dwg



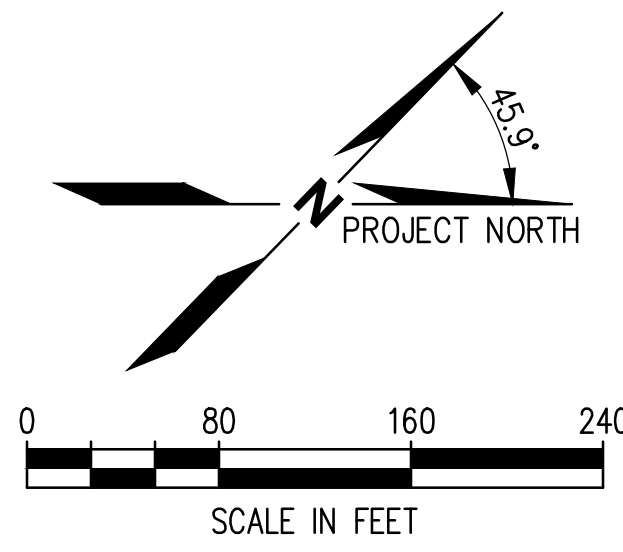
FUEL BARGE & AML BARGE VESSEL MOORING NOTES

- CONTRACTOR SHALL PROVIDE FUEL BERTHING AND MOORAGE DURING CONSTRUCTION. CONTRACTOR SHALL KEEP THE SLIP OPEN FOR FUEL BARGE AND AML BARGE ACCESS WHEN SCHEDULED.
- FUEL BARGE IS GENERALLY EXPECTED EVERY 20 DAYS. FUEL TRANSFER DURATION IS TYPICALLY 36-48 HOURS. AML BARGES TYPICALLY LOAD WEEKLY SUNDAY-MONDAY AND CAN VARY. CONTRACTOR TO COORDINATE WITH MOS ON SCHEDULING. CONTRACTOR SHALL KEEP THE ORE BASIN OPEN FOR AML BARGE TRAFFIC WHEN SCHEDULED.
- NO HOT WORK WILL BE ALLOWED WITHIN 300' OF THE FUEL BARGE WHILE IN PORT. NO CRANE WORK WITHIN RANGE OF THE BARGE WILL BE PERMITTED.
- NEW AND EXISTING STRUCTURES SHOWN IN THIS VIEW, CONTRACTOR SHALL DETERMINE PHASING OF PROJECT TO ALLOW FOR FUEL BARGE OPERATIONS AND AML BARGE OPERATIONS.
- CONTRACTOR SHALL SUBMIT FUEL BARGE MOORING AND BERTHING PLANS 3 WEEKS PRIOR TO FUEL BARGE ARRIVAL.
- CONTRACTOR SHALL CONFIRM WITH MOS AND PETRO-MARINE THAT ALL FUEL LINES HAVE BEEN EMPTIED PRIOR TO RESUMING HOT WORK AND CRANE OPERATIONS IN THE FUEL LINE AND HEADER VICINITY. SEE G SHEETS FOR PETRO-MARINE CONTACT INFORMATION.
- CONTRACTOR MAY MOOR VESSELS AND BARGES AT BROADWAY DOCK WITH FLOATING FENDERS REMOVED BY CONTRACTOR TO PREVENT DAMAGE. FLOATING FENDERS SHALL BE REMOVED FOR FUEL BARGE OPERATIONS.
- NOTE EXISTING BERTH LINE AND CURRENT BERTH LINE DIFFER IN LOCATION.
- TEMPORARY FUEL HEADER. CONTRACTOR DESIGNED, CONTRACTOR TO PROVIDE CONNECTIONS FOR FUEL LINES. SEE SHEET G8.06.
- CONTRACTOR MAY TEMPORARILY RELOCATE THE PLUMB PILE FROM BARGE DOLPHIN 3 TO ACCOMMODATE AML BARGE BERTHING DURING CONSTRUCTION OF DOLPHIN 8.

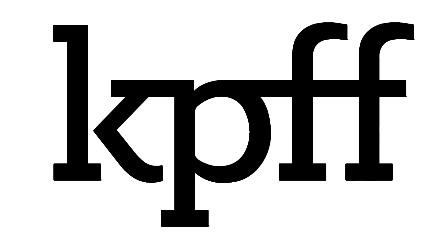
OVERALL SITE PLAN
SCALE: 1" = 80'

LEGEND

- EXISTING STRUCTURES TO BE REMOVED, SEE DEMOLITION SHEETS
- EXISTING FUEL LINES
- FUEL LINES
- SECURITY FENCE



TEMP FUEL LINE RE-SCOPE 7.7.2023



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Seattle, Washington 98101
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NO.	DATE	BY	REVISION
1	05/18/23	JLF	ADDENDUM 1
2	06/02/23	EJD	ADDENDUM 3



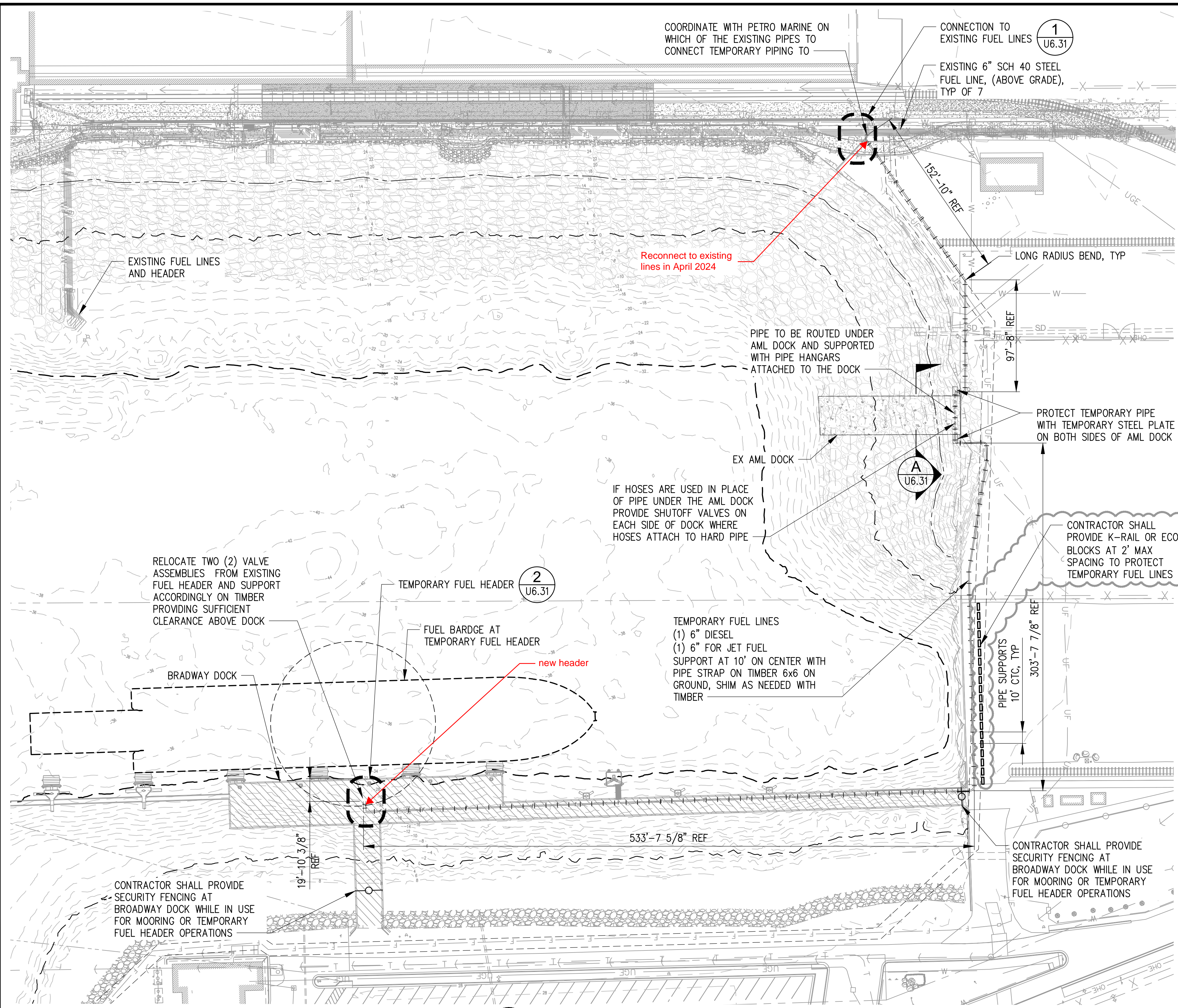
ORE PENINSULA REDEVELOPMENT
SKAGWAY, ALASKA

OVERALL SITE PLAN - CONSTRUCTION
FUELING AND AML BARGE OPERATIONS

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: RR	DATE: 05/04/2023
DRAWING NO.	G8.05
SHEET NO.	14 OF 374

BID SET - NOT FOR CONSTRUCTION

Plotted: May 18, 2023 - 1:00pm dyu Layout: U6.30
 M:\2021\2100135 Skagway Ore Peninsula Multi-Use Dock Drawings\Current\2100135_U6.30 Temp Fuel Line Plan.dwg

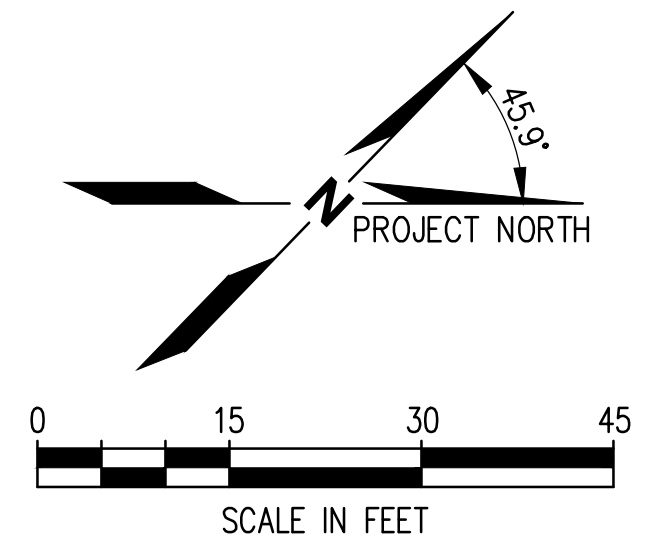


NOTES

- NEW STEEL FUEL PIPE SHALL BE ASTM A53 GRADE B, 6" SCHEDULE 40, COATED PER SPECIFICATIONS, BUT NOT GALVANIZED. JOINT SHALL BE BUTT WELDED.
- WHERE PIPE SUPPORTS ARE USED THESE SHALL BE HOT DIPPED GALVANIZED OR 316 STAINLESS STEEL. TYPICAL MAXIMUM SPACING SHALL BE 10' FOR 6" PIPE. PIPE STRAPS TO SECURE PIPES TO TIMBER UNLESS NOTED. PIPE STRAPS SHALL BE GALVANIZED OR COATED. PIPE STRAPS SHALL NOT CAUSE DAMAGE TO PIPE COATING.
- MAINTAIN 18" MINIMUM CLEARANCE BELOW BOTTOM OF PIPE, FLANGE, OR VALVE AND 6" MINIMUM CLEARANCE ABOVE TOP OF PIPE, FLANGE, OR VALVE TO PROVIDE CLEARANCE FOR INSTALLATION AND REMOVAL.
- EXISTING FUEL LINE SHALL BE CUT AND REMOVED IN ACCORDANCE WITH THE UTILITY DEMOLITION SPECIFICATIONS.
- CONTRACTOR TO PROVIDE TEMPORARY FLANGE JOINTS AND HOSES AS NECESSARY TO MAINTAIN FUEL SERVICE AT ALL PHASES OF CONSTRUCTION.
- SEE UPLAND PLANS FOR UPLAND GRADING DETAILS.
- BASED ON THESE DRAWINGS, THE CONTRACTOR SHALL DEVELOP DETAILED SHOP DRAWINGS THAT PROVIDE A FULLY FUNCTIONAL ARRANGEMENT SUITABLE FOR INSTALLATION, TAKING INTO ACCOUNT SITE CONDITIONS. DIMENSIONS SHALL BE VERIFIED FROM SITE AND MANUFACTURERS' CERTIFIED DRAWINGS. THESE DRAWINGS ARE DIAGRAMMATIC AND DO NOT REPRESENT A COMPLETE DETAILED DESIGN. EQUIPMENT LAYOUT IS APPROXIMATE.
- COMPLY WITH THE APPLICABLE LOCAL, STATE, AND NATIONAL CODES, ORDINANCES AND REGULATIONS AFFECTING MATERIALS AND METHODS OF INSTALLATION OF THE FUEL SYSTEMS. FOLLOW RECOMMENDED PRACTICES AS SET FORTH BY ASME, 2012 INTERNATIONAL BUILDING CODE, 2012 INTERNATIONAL MECHANICAL CODE, 2012 UNIFORM PLUMBING CODE, GA, AND OSHA, AS THEY APPLY TO THIS PROJECT, EXCEPT IN CASES WHERE STATUTES GOVERN.
- PIPING SHALL BE RUN AS DIRECTLY AS PRACTICABLE WITH A MINIMUM NUMBER OF BENDS. SUFFICIENT CLEARANCES SHALL BE PROVIDED FOR REPLACEMENT OF ALL VALVING. PROVIDE SUFFICIENT CLEARANCE ABOVE, BELOW, AND BETWEEN PIPES. PROVIDE ADEQUATE LOOPS OR OTHER MEASURES AS NEEDED TO ALLOW FOR THERMAL EXPANSION/CONTRACTION.
- UPON COMPLETION OF THE FUEL SYSTEMS INSTALLATION, EXAMINATION, INSPECTION AND ALL TESTING SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF ASME B31.4 AND OF THE AUTHORITY HAVING JURISDICTION.
- DEMONSTRATE TO THE PROJECT ENGINEER'S SATISFACTION THAT THE SYSTEMS HAVE BEEN INSTALLED IN A SATISFACTORY MANNER IN ACCORDANCE WITH THE PLANS, SPECIFICATIONS, AND APPLICABLE CODES. DEMONSTRATE DYNAMIC OPERATION OF ALL SYSTEMS AND THAT ALL EQUIPMENT OPERATES PROPERLY.
- COORDINATE INSTALLATION OF TEMPORARY PIPING ACCORDINGLY WITH OTHER CONSTRUCTION WORK HAPPENING IN THE AREA AND TO THE SATISFACTION OF THE PROJECT ENGINEER.
- FOR ADDITIONAL NOTES AND DETAILS SEE G8.05

LEGEND

- EXISTING 6" SCH 40 STEEL FUEL LINE
- 6" SCH 40 STEEL FUEL LINE



TEMP FUEL LINE RE-SCOPE 7.7.2023

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STATE OF ALASKA

DANIEL W. HARTFORD
 No. ME11178
 PROFESSIONAL ENGINEER
 5/04/2023

NO.	DATE	BY	REVISION
1	05/18/23	JLF	ADDENDUM 1

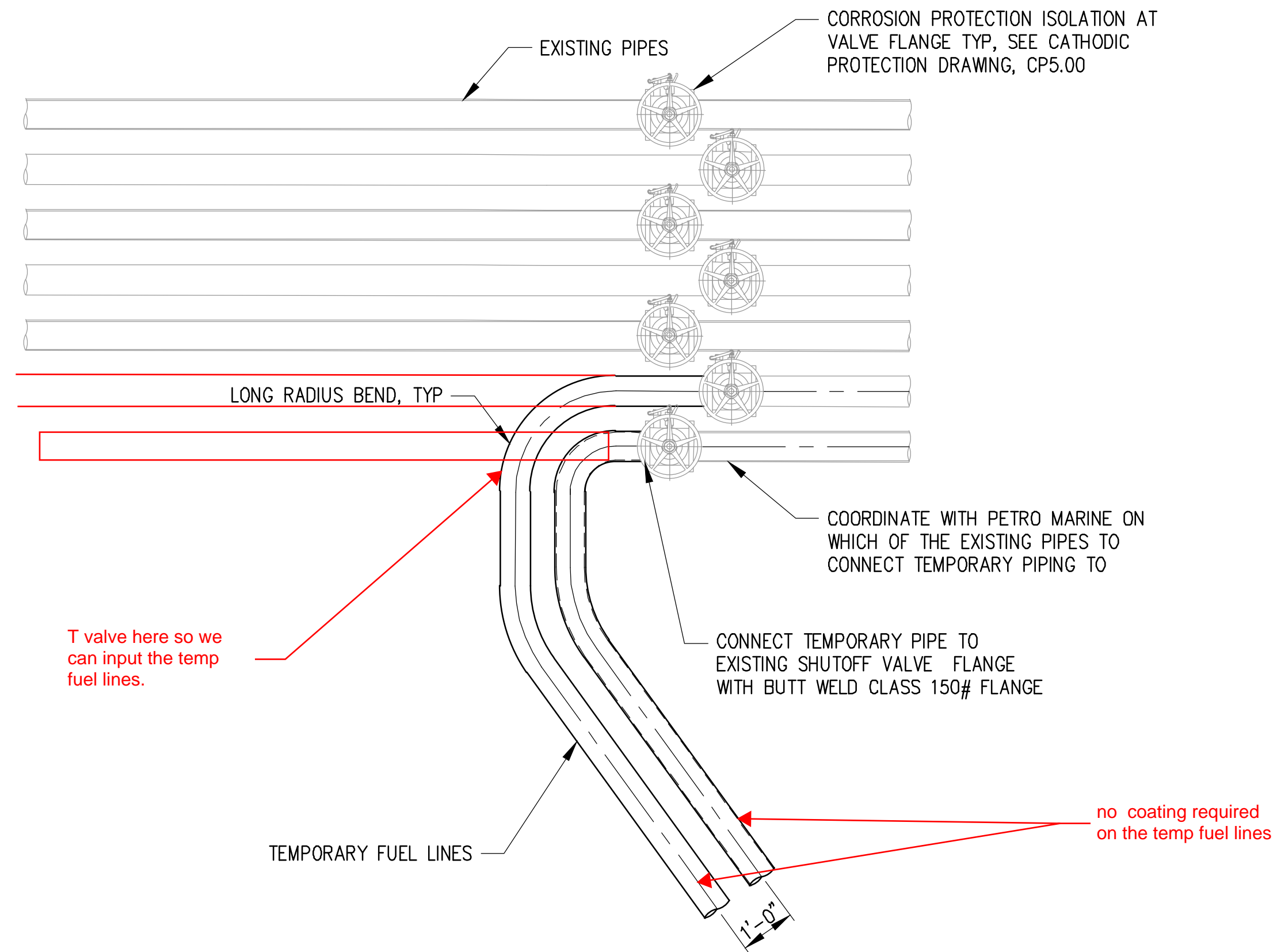


ORE PENINSULA REDEVELOPMENT
 SKAGWAY, ALASKA

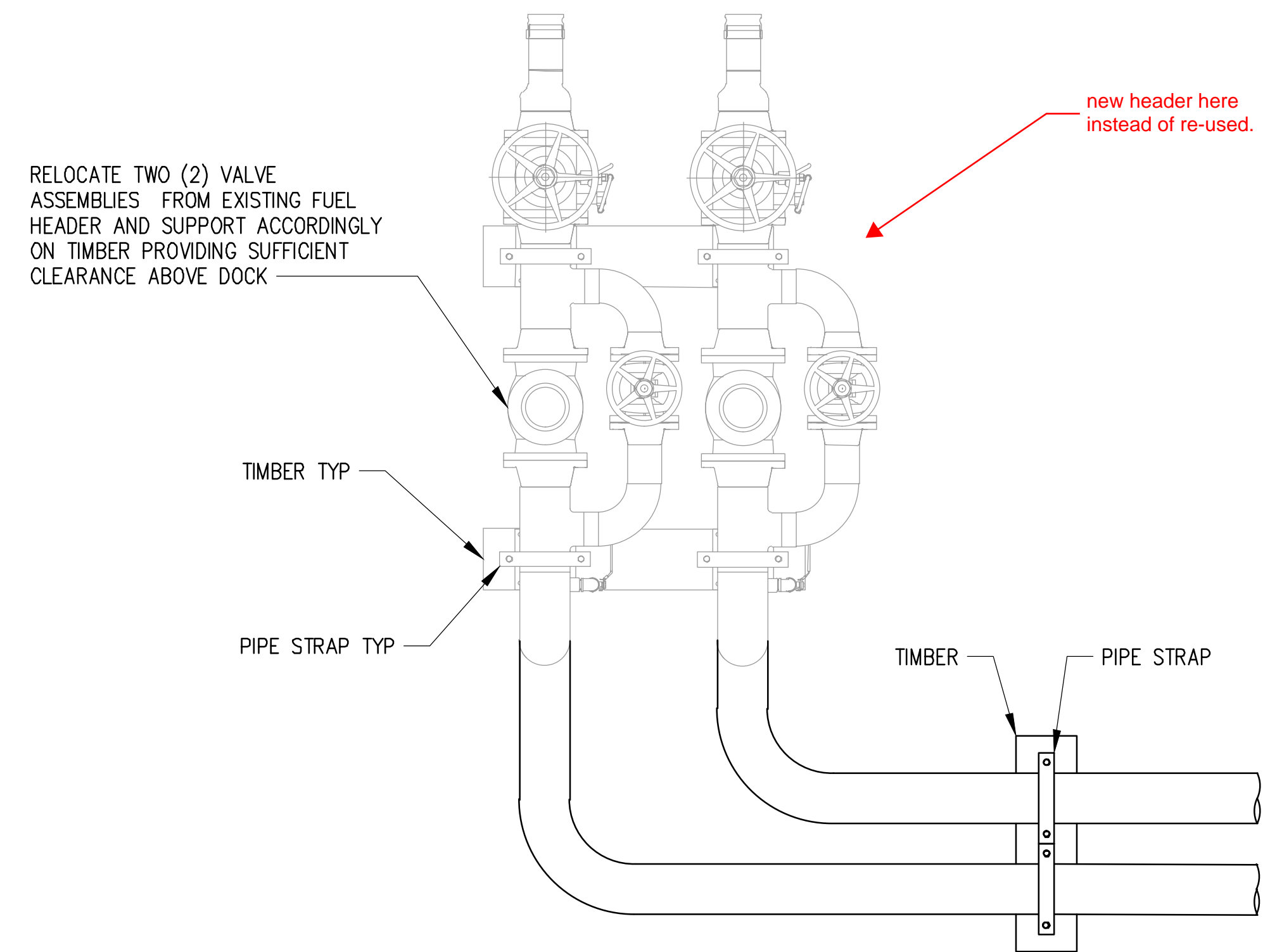
TEMP FUEL LINE PLAN

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: DWH	DATE: 05/04/2023
DRAWING NO.	U6.30
SHEET NO. 161	OF 374

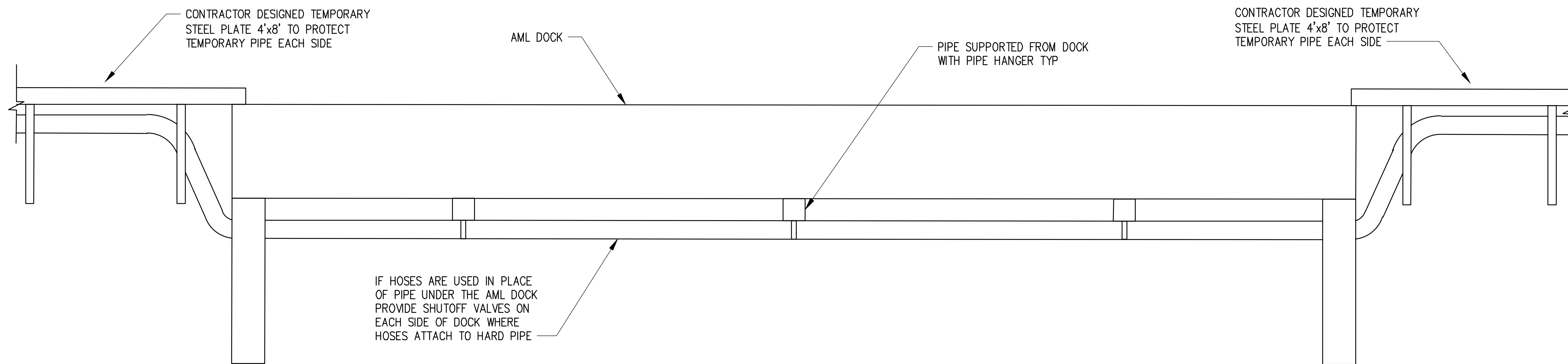
BID SET - NOT FOR CONSTRUCTION



1 CONNECTION TO EXISTING FUEL LINES
U6.30 SCALE: NTS



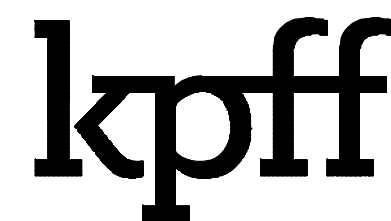
2 TEMPORARY FUEL HEADER
U6.30 SCALE: NTS



A SECTION - UNDERNEATH DOCK
U6.30 SCALE: NTS

TEMP FUEL LINE RE-SCOPE 7.7.2023

Plotted: May 08, 2023 - 11:49am dju Layout: U6.31
M:\2021\2100135 Skagway Ore Peninsula Multi-Use Dock Drawings\Current\2100135_U6.31 Temp Fuel Line Details.dwg



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NO.	DATE	BY	REVISION



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TEMP FUEL LINE DETAILS

DRAWN: JH	PROJECT NO.: 2100135
DESIGN: ED	SCALE: AS SHOWN
CHECKED: DWH	DATE: 05/04/2023
DRAWING NO.	U6.31
SHEET NO.	162 OF 374

BID SET - NOT FOR CONSTRUCTION

Brett Gunderson

From: Dan Hartford <dan.hartford@kpff.com>
Sent: Friday, July 7, 2023 11:10 AM
To: Brett Gunderson; Ed DeBroeck
Cc: Chris Willis; Luke Parham
Subject: RE: Temporary Fuel Valves

CAUTION: This email originated from outside Haskell Corporation. Do not click links or open attachments unless you are sure the sender is valid and the content is safe.

The proposed valves are acceptable for this application.

Regards,
Dan



Dan Hartford, PE, P.Eng

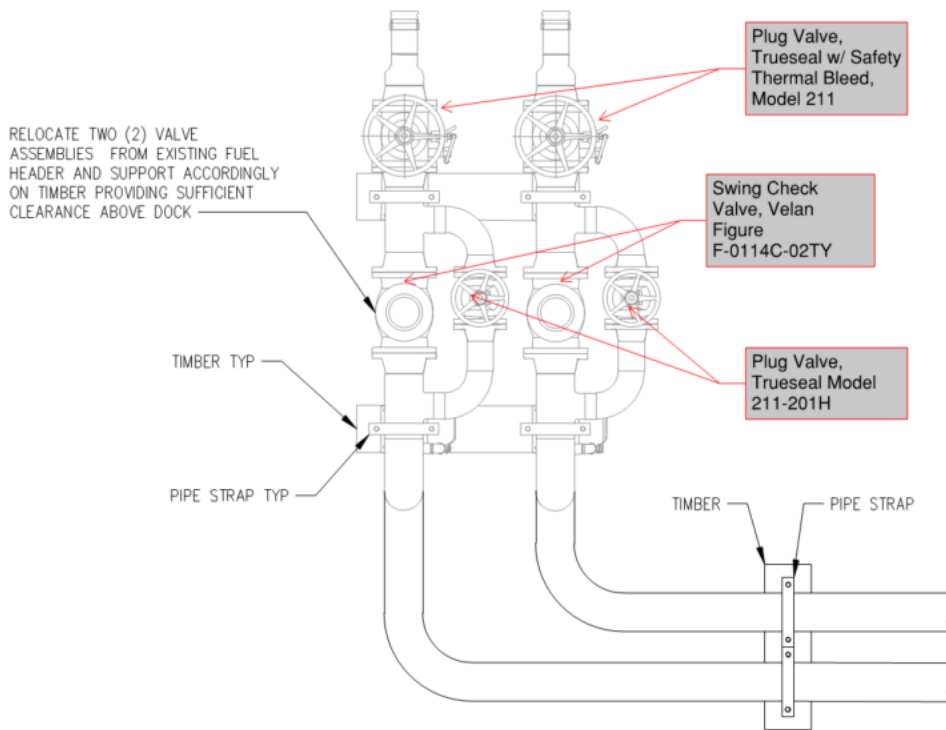
Associate

[O 206.382.0600](tel:206.382.0600) [D 206.388.1551](tel:206.388.1551) [M 425.985.6728](tel:425.985.6728)
1601 Fifth Avenue, Suite 1300
Seattle, WA 98101

From: Brett Gunderson <bgunderson@haskellcorp.com>
Sent: Friday, July 7, 2023 10:13 AM
To: Dan Hartford <dan.hartford@kpff.com>; Ed DeBroeck <ed.debroeck@kpff.com>
Cc: Chris Willis <chrisw@pacificpile.com>; Luke Parham <lparham@haskellcorp.com>
Subject: Temporary Fuel Valves

Ed, Dan,

For the header valves, can I assume the following:



2 TEMPORARY FUEL HEADER
 U6.30 SCALE: NTS

I have current pricing on these valves as they were the basis for the new headers in the bid. I would also like to use the 6" Trueseal Model 211 at the tie-in points off of the tee, and again at the isolation point at the shore side of the Broadway Dock.

Let me know if this is acceptable.

Brett Gunderson | **HASKELL CORPORATION** | Project Manager/Estimator | www.haskellcorp.com
 Office 360.676.7226 | Mobile 360.483.8188 | Fax 360.734.5538 | bgunderson@haskellcorp.com