



To: Brad Ryan, Borough Manager, Municipality of Skagway  
From: Ross Hunnicutt, Construction Project Manager, KPFF Consulting Engineers  
Cc: Cody Jennings, Emily Deach, Ed DeBroeck, Rob Price

RE: Ore Peninsula Redevelopment Project, Pacific Pile & Marine  
**Change Order No. 006**

Date: April 24<sup>th</sup> 2024

**Summary:** On July 25, 2023, The Municipality of Skagway (MOS) and Pacific Pile & Marine (PPM) entered into a contract for the Ore Dock Redevelopment Project. Below is a summary of the accepted changes in price to the Ore Dock Redevelopment Project to date, and Change Order 006.

**Original Contract Price** – \$39,402,785.48 as specified and accepted in Exhibit A of the Contract. The authorized total contract including Force Account allowances is \$41,196,879.60.

Change Order No. 001 - \$ 125,280.49 approved October 19, 2023.  
Change Order No. 002 - \$ 26,476.08 approved December 21, 2023.  
Change Order No. 003 - \$ 15,627.67 approved January 4<sup>th</sup>, 2024.  
Change Order No. 004 - \$ 35,982.42 approved February 15<sup>th</sup>, 2024.  
Change Order No. 005 - \$ 164,791.55 approved March 7<sup>th</sup>, 2024.

Field Order No. 002 - \$ 300,000 approved November 20<sup>th</sup>, 2023  
Field Order No. 003 - \$ 244,568.42 approved December 21, 2023  
Field Order No. 007 - \$ 327,736.66 approved March 7<sup>th</sup>, 2024  
Field Order No. 008 - \$ 359,035.33 approved March 21<sup>st</sup>, 2024

**Current Contract Price - \$42,796,378.22**

**Original Contract Schedule** – PPM shall have the base bid Work defined in Exhibit A of the Contract completed no later than the Substantial Completion Milestone 1 Date. No Change to contract schedule included in this PCO.

**Scope of Changes (Change Order 006):**

PCO 013 – Drawing Revision 05 – Salvage Light Poles	Additive cost \$ 1,438.53
PCO 032 – Ore Platform Beam Delamination	Additive cost \$ 52,911.42
PCO 034 – CD Trestle Pile Coating	Additive cost \$ 8,182.65
PCO 037 – Access Ramp Transition Plate Testing	Additive cost \$ 4,749.14
PCO 044 – Dolphin Ladder Modifications	Additive cost \$ 17,410.18

Total Cost Impacts associated with Change Order 006: \$ 84,691.92  
Schedule Impact of this change(s): 0 Days



**Chang Order 006 contains the following project changes:**

**PCO 013 – Drawing Revision 05, Salvage Light Poles - Additive cost \$ 1,438.53**

KPFF issued a Drawing Revision requiring salvaging existing light poles and navigation light in lieu of demolition because of descoping the electrical portion of the project. The light pole salvage was a no cost change, re-attaching the navigation light to the new dolphin structure was performed on a Time and Materials basis for the cost of \$1,438.53 because of this change.

**PCO 032 – Ore Platform Beam Delamination - Additive cost \$ 52,911.42**

During demolition of the Ore Platform, PPM and KPFF observed the Ore Loader Platform concrete support beams separated from the upper platform structure and risked major failure during removal. PPM added additional box beam reinforcing and procured additional engineering design support to prevent a potential failure of the platform structure from the observed beam behavior. During concrete destruction, there was a difference in the physical location of reinforcing compared to what was indicated on the Record Drawings. This difference in physical condition warranted the extra mitigation taken by PPM during demolition to support the concrete platform. PCO 032 includes additional engineering, materials, additional labor and equipment costs to perform the mitigations.

**PCO 034 – CD Trestle Pile Coating - Additive cost \$ 8,182.65**

Piles at the Cruise Dock Access Trestle driven to the design criteria did not have epoxy coating at the mudline elevation, and PPM added epoxy coating in place to the exposed steel pile sections at the Engineer's direction. The price for performance was mutually accepted by KPFF and PPM.

**PCO 037 – Access Ramp Transition Plate Testing - Additive cost \$ 4,749.14**

Upon final observations of the Cruise Dock Access Ramp at the fabrication shop in Washington, the Engineer directed KPFF to perform a range of motion test to ensure movement of the Access Ramp Transition Plates met proper function when installed in Skagway. The Engineer directed PPM and Jesse Engineering to perform a minor modification in the fabrication shop to the Access Ramp in Seattle to mitigate any potential conflict with transition plate range of motion. PCO 037 costs include labor and equipment from Jesse Engineering, and additional swivel hooks.

**PCO 044 – Dolphin Ladder Modifications - Additive cost \$ 17,410.18**

The Dolphin 6 and 7 ladder angles built per the approved shop drawings were too short to weld directly to the dolphin bottom plate during installation. Angle iron was welded from the ladder angle to the top of the bottom plate as a field solution to mitigate schedule and cost impacts as directed by KPFF. PCO 044 costFAs include materials, labor, and equipment for work completed.

**Ore Peninsula Redevelopment Project**

**OWNER:** Municipality of Skagway  
**DESC:** PCO 044 - Dolphin Ladder Mods

**START DATE:** April 13, 2024  
**FINISH DATE:** April 19, 2024

**COST SUMMARY**

DESCRIPTION OF WORK	COST TYPE	QUANT.	UNITS	UNIT RATE	LABOR (LAB)	SUPPLIES (SUP)	PPM EQUIP (PPM)	RENT EQUIP (RENT)	SUBCONT (SUB)	PERM MAT'L (PM)
<b>Pacific Pile &amp; Marine</b>										
Labor	LAB	1.0	LS	\$5,226.28	\$5,226					
Equipment-Owned	PPM	1.0	LS	\$1,687.32			\$1,687			
Equipment-Rental	RENT	1.0	LS	\$7,470.89				\$7,471		
Materials	PM	1.0	LS	\$298.68						\$299
<b>Subcontractors</b>										
DAMA Industrial	SUB	1.0	LS	750.00					\$750	
<b>SUBTOTALS:</b>					LABOR	SUPPLIES	PPM EQUIP	RENT EQUIP	SUBS	PERM MATERIAL
					\$5,226.28	\$0.00	\$1,687.32	\$7,470.89	\$750.00	\$298.68
Taxes @: 0.00%										
OH and Fee					15.0%	10.0%	10.0%	10.0%	10.0%	10.0%
Markup Totals					\$783.94	\$0.00	\$168.73	\$747.09	\$75.00	\$29.87
<b>ITEM TOTALS</b>					\$6,010.22	\$0.00	\$1,856.05	\$8,217.98	\$825.00	\$328.55

PPM reserves the right to request additional schedule time due to impacts to the critical path schedule associated with this PCO.

<b>TOTAL AMOUNT</b>	<b>\$17,237.80</b>
<b>Bond</b>	<b>1.0% \$172.38</b>
<b>SUBTOTAL</b>	<b>\$17,410.18</b>
<b>GRAND TOTAL</b>	<b>\$17,410.18</b>



**Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009**

Date Work Performed: 4/13/2024

Description: D6 and D7 Ladder Modifications

Cut/weld angle for attaching lower D6 and D7 ladder angle to bottom cap plates

Phase Code: \_\_\_\_\_

Craft Code	QUANTITY			UOM	NOTES
	RT	OT	DT		
<b>LABOR</b>	<b>HR</b>	<b>HR</b>	<b>HR</b>		
		4			Cut/weld ladder angle
		4			Crane support/move equipment/material
		4			Ladder crane support
		4			Cut/weld ladder angle
		4			Supervision
	EQ #				
<b>EQUIPMENT</b>					
Demag 2500 Crane		2			
400A Welding Machine		4			
S85 Manlift		4			
	INVOICE	QTY	UNIT		
<b>MATERIALS</b>					
Welding/Cutting					
Consumables		4	HR		
3x3x1/4 Angle Iron		4	LF		
	INVOICE	QTY	UNIT		
<b>SUBCONTRACTORS</b>					
	INVOICE	QTY	UNIT		
<b>SERVICES</b>					

OWNER REPRESENTATIVE

*William Hammann*

4-14-2024

PPM REPRESENTATIVE

*[Signature]*

Date 4-14-24

Date





Force Account Worksheet- Salvage Handrail

**Work Affected:** 4/16/24 - Cut/weld angle for attaching lower D3 ladder angle to bottom cap plate. Coat D6/7 ladder mods

**Date:** 4/16/2024

**FA Submittal No.:** PCO 044 - Dolphin Ladder Modifications

**LABOR**

EMPLOYEE NAME	CLASS	PER DIEM	HRS	RATE	COST
	OE Superintendent I	\$7.92	1.0	\$107.70	\$115.62
	PD Foreman	\$21.83	2.0	\$88.70	\$199.23
	PD Foreman	\$21.83	2.0	\$88.70	\$199.23
	PD Journeyman Welder	\$10.92	1.0	\$82.02	\$92.94
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
<b>TOTALS:</b>		<b>\$0.00</b>	<b>0.0</b>		<b>\$607.02</b>

**Rate Description:**

**Labor Clarifications**  
Craft Labor classifications per certified payroll.  
Staff labor classifications are fully burdened.  
Per diem costs (housing and meals) are prorated per a 12 hour working day based on actual housing costs during time of work.  
Labor rates are fully burdened costs (Base rate + tax + union contributions + Workers compensation).  
Straight/Overtime/Doubletime rates included as applicable to the day (weekday/weekend/holiday) and time of day of work performed.

**Equipment Clarifications**  
Owned equipment rates per Rental Rate Blue Book - Alaska South region adjustment.  
Markups per 00 72 00 11.4.A. of the Contract.

**\$607.02**

MARKUP @: 15% \$91.05  
SUBTOTAL: \$698.07

**EQUIPMENT**

EQUIP. DESCRIPTION	WORKING TIME			STANDBY TIME			TOTAL
	HRS	RATE	COST	HRS	RATE	COST	
Pacific Lifter (Rental)	1.0	\$1,391.67	\$1,391.67			\$0.00	\$1,391.67
400A Welding Machine	2.0	\$7.57	\$15.14			\$0.00	\$15.14
Jilly Basket	1.0	\$0.00	\$0.00			\$0.00	\$0.00
S85 Manlift (Rental)	1.0	\$73.22	\$73.22			\$0.00	\$73.22
			\$0.00			\$0.00	\$0.00
			\$0.00			\$0.00	\$0.00
<b>TOTALS:</b>	<b>5.0</b>		<b>\$1,480.03</b>	<b>0.0</b>		<b>\$0.00</b>	<b>\$1,480.03</b>

MARKUP @: 10.0% \$148.00  
SUBTOTAL: \$1,628.03

**SUBCONTRACTOR & MATERIAL INVOICING**

NAME	DESCRIPTION	Work Impacted	TOTAL
Welding Consumables	2 HR at \$21/hr		\$42.00
4x4x1/2 Angle Iron	4 LF at \$6/FT		\$24.00
DAMA	Coating Sub - 1hr at \$250/hr		\$250.00
<b>TOTALS:</b>			<b>\$316.00</b>

MARKUP @: 10.0% \$31.60  
SUBTOTAL: \$347.60



SUMMARY OF COSTS				
Cost		Markup		Totals
LABOR:	\$607.02	+	\$91.05	= \$698.07
EQUIP:	\$1,480.03	+	\$148.00	= \$1,628.03
MATERIALS:	\$316.00	+	\$31.60	= \$347.60
<b>TOTALS:</b>	<b>\$2,403.05</b>	<b>+</b>	<b>\$270.66</b>	<b>= \$2,673.70</b>

This T&M sheet represents the direct cost associated with the activities on the date noted at the top of the sheet. Cumulative impact on project schedule and cost is not included within this breakdown and will be evaluated separately.

PPM Representative: \_\_\_\_\_  
MOS Representative: \_\_\_\_\_



Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009

Date Work Performed: 4/16/2024

Description: D3,6,7 Ladder Modifications

Cut/weld angle for attaching lower D3 ladder angle to bottom cap plates. Coat D6/7 ladder mods.

Phase Code: \_\_\_\_\_

Craft Code	QUANTITY			UOM	NOTES
	RT	OT	DT		
<b>LABOR</b>	<b>HR</b>	<b>HR</b>	<b>HR</b>		
	1				Crane support
	2				Supervision
	2				Cut/weld ladder angle
	1				Cut/weld ladder angle
	EQ #				
<b>EQUIPMENT</b>					
Pacific Lifter	1				
400A Welding Machine	2				
S85 Manlift	1				Coating access
Jilly	1				
	INVOICE	QTY	UNIT		
<b>MATERIALS</b>					
Welding/Cutting					
Consumables	2		HR		
4x4x1/2 Angle Iron	4		LF		
	INVOICE	QTY	UNIT		
<b>SUBCONTRACTORS</b>					
DAMA	1		HR		Coat D6/7 ladder mods
	INVOICE	QTY	UNIT		
<b>SERVICES</b>					

OWNER REPRESENTATIVE

*William Hammar* 4-16-2024

PPM REPRESENTATIVE

*[Signature]* 4-16-24

Date

Municipality of Skagway  
Ore Peninsula Redevelopment Project

Force Account Worksheet- Salvage Handrail

**Work Affected:** 4/19/24 - Cut/weld angle for attaching lower D1 ladder angle to bottom cap plate. Coat D1/2/3 ladder mods

**Date:** 4/16/2024

**FA Submittal No.:** PCO 044 - Dolphin Ladder Modifications

**LABOR**

EMPLOYEE NAME	CLASS	PER DIEM	HRS	RATE	COST
	OE Superintendent I	\$23.75	3.0	\$147.90	\$467.45
	PD Foreman	\$21.83	2.0	\$118.90	\$259.63
	PD Journeyman Welder	\$34.00	2.0	\$108.86	\$251.72
	PD Journeyman Welder	\$21.83	2.0	\$108.86	\$239.55
	PD Journeyman Welder	\$21.83	2.0	\$108.86	\$239.55
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
<b>TOTALS:</b>		<b>\$0.00</b>	<b>0.0</b>		<b>\$1,457.91</b>

**Rate Description:**

**Labor Clarifications**  
 Craft Labor classifications per certified payroll.  
 Staff labor classifications are fully burdened.  
 Per diem costs (housing and meals) are prorated per a 12 hour working day based on actual housing costs during time of work.  
 Labor rates are fully burdened costs (Base rate + tax + union contributions + Workers compensation).  
 Straight/Overtime/Doubletime rates included as applicable to the day (weekday/weekend/holiday) and time of day of work performed.

**Equipment Clarifications**  
 Owned equipment rates per Rental Rate Blue Book - Alaska South region adjustment.  
 Markups per 00 72 00 11.4.A. of the Contract.

**\$1,457.91**

MARKUP @: 15% \$218.69  
 SUBTOTAL: \$1,676.59

**EQUIPMENT**

EQUIP. DESCRIPTION	WORKING TIME			STANDBY TIME			TOTAL
	HRS	RATE	COST	HRS	RATE	COST	
Pacific Lifter (Rental)	3.0	\$1,391.67	\$4,175.01			\$0.00	\$4,175.01
400A Welding Machine	2.0	\$7.57	\$15.14			\$0.00	\$15.14
Jilly Basket	2.0	\$0.00	\$0.00			\$0.00	\$0.00
S85 Manlift (Rental)	2.0	\$73.22	\$146.44				\$146.44
			\$0.00				\$0.00
			\$0.00				\$0.00
<b>TOTALS:</b>	<b>9.0</b>		<b>\$4,336.59</b>	<b>0.0</b>		<b>\$0.00</b>	<b>\$4,336.59</b>

MARKUP @: 10.0% \$433.66  
 SUBTOTAL: \$4,770.25

**SUBCONTRACTOR & MATERIAL INVOICING**

NAME	DESCRIPTION	Work Impacted	TOTAL
Welding Consumables	2 HR at \$21/hr		\$42.00
4x4x1/2 Angle Iron	4 LF at \$6/FT		\$24.00
DAMA	Coating Sub - 2hr at \$250/hr		\$500.00
<b>TOTALS:</b>			<b>\$566.00</b>

MARKUP @: 10.0% \$56.60  
 SUBTOTAL: \$622.60



SUMMARY OF COSTS				
Cost		Markup		Totals
LABOR:	\$1,457.91	+	\$218.69	= \$1,676.59
EQUIP:	\$4,336.59	+	\$433.66	= \$4,770.25
MATERIALS:	\$566.00	+	\$56.60	= \$622.60
<b>TOTALS:</b>	<b>\$6,360.50</b>	<b>+</b>	<b>\$708.94</b>	<b>= \$7,069.44</b>

This T&M sheet represents the direct cost associated with the activities on the date notated at the top of the sheet. Cumulative impact on project schedule and cost is not included within this breakdown and will be evaluated separately.

PPM Representative: \_\_\_\_\_  
 MOS Representative: \_\_\_\_\_

Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009

Date Work Performed: 4/19/2024

Description: D1,2,3 Ladder Modifications

Cut/weld angle for attaching lower D1 ladder angle to bottom cap plates. Coat D1,2,3 ladder mods.

Phase Code: \_\_\_\_\_

Craft Code	QUANTITY			UOM	NOTES
	RT	OT	DT		
<b>LABOR</b>	<b>HR</b>	<b>HR</b>	<b>HR</b>		
		3			Crane support
		2			Supervision
		2			Cut/weld ladder angle
		2			Cut/weld ladder angle
		2			Coating support
	EQ #				
<b>EQUIPMENT</b>					
Pacific Lifter		3			
400A Welding Machine		2			
S85 Manlift		2			
Jilly		2			
	INVOICE	QTY	UNIT		
<b>MATERIALS</b>					
Welding/Cutting					
Consumables		2	HR		
4x4x1/2 Angle Iron		4	LF		
	INVOICE	QTY	UNIT		
<b>SUBCONTRACTORS</b>					
DAMA		2	HR		Coat D1,2,3 ladder mods
	INVOICE	QTY	UNIT		
<b>SERVICES</b>					

OWNER REPRESENTATIVE

*William Hemmer* 4-19-2024

PPM REPRESENTATIVE

*[Signature]* 4-19-24

Date



Pacific Pile & Marine, LP  
700 South Riverside Drive  
Seattle, WA 98108-4364

T 206 331-3873  
F 206 774-5958  
License # PACIFPM922J3

April 17<sup>th</sup>, 2024

Serial Letter 074

KPFF  
1601 Fifth Avenue, Suite 1300  
Seattle, WA 98101

Attention: Ed DeBroeck, Resident Engineer

Project: Ore Peninsula Redevelopment Project, Skagway, AK

Subject: DSC 001 Supporting Documentation – Ore Platform Delamination

Reference: RFI 102 – Ore Loader Platform Concrete Demolition, dated 11/19/23  
SL 016 – Notice of DSC – Concrete Platform Beam Delamination, dated 11/26/23

Mr. DeBroeck,

This letter is written to provide a summary of the documentation provided to support the presence of the Differing Site Condition observed while demolishing the ore loader platform concrete.

Photos were provided on 11/18/23 to KPFF regarding delamination of the beams along the soffit of the ore loader platform during demolition.

Please reference RFI 102, dated 11/19/23 for documentation of the original discovery of the differing site condition, the delamination of the beams along the soffit of the ore loader platform during demolition.

To mitigate risk of an uncontrolled separation of the delaminating beam, PPM engaged a third-party engineer, BMA Construction Engineers, to engineer beam supports. Calculations for these supports are provided within the RFI 102 VPO file.

KPFF and PPM met on 11/22/23 and confirmed the engineered solution by Brian Mapel was satisfactory.

The issue caused a delay for removal of ore loader platform concrete. PPM has contracted CEI to perform additional core drilling to facilitate installation of the box beam brackets by PPM. Once the concrete beams were secured, CEI continued to wire saw the concrete platform. T&M tickets for PPM labor and the CEI invoice are included in the PCO 032 VPO file.

Appendix D Historical Drawings Loader Platform Details S6 and S7 show #7 epoxy coated dowels spaced at 8' on center to support Beam No. 1 and 2 under the ore loader platform.

PPM's third-party engineer, BMA Construction Engineers, provided calculations exhibiting the beams should have been self-supporting during the demolition of the platform provided the #7 dowels were installed per the Historical Drawings. Calculations are included in the RFI 102 VPO file.

PPM coordinated a phased demolition of the concrete on shore to allow KPFF's QA representative to investigate the concrete and measure the dowel spacing.



Pacific Pile & Marine, LP  
700 South Riverside Drive  
Seattle, WA 98108-4364

T 206 331-3873  
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License # PACIFPM922J3

The QA inspection report dated 3/27/24 noted two #7 bars were observed with placement 8' on center. However, QA inspection reports dated 3/28/24 and 4/6/24 noted #7 bars with placement at 12'-6" and 10' on center, respectively.

The #7 bars are not consistently installed at 8' on center per the Historical Drawings. The delamination observed was a result of a differing site condition as the beams should have been self-supporting during the demolition of the platform provided the #7 dowels were installed per the Historical Drawings

Please contact the undersigned at (206) 640-6578 or at [Nicolee@pacificpile.com](mailto:Nicolee@pacificpile.com) with questions regarding this issue.

Sincerely,

Pacific Pile & Marine

A handwritten signature in blue ink, appearing to read 'Nicole Egli', is written over a light blue circular stamp.

Nicole Egli  
Project Manager

Attachments:

1. RFI 102 - Ore Loader Platform Concrete Delamination
2. PCO 032 - Ore Platform Delamination
3. Appendix D Historical Drawings & Reports
4. Pacific CM IDR 3/27/24
5. Pacific CM IDR 3/28/24
6. Pacific CM IDR 4/6/24



### 102 - Ore Loader Platform Concrete Delamination

No. 102

CREATED	RESPONDED	DUE DATE	ANSWERED
11/19/2023	11/29/2023	12/15/2023	

<b>TO</b> KPFF-Ed DeBroeck  	<b>LOCATION</b>  <b>OWNER</b> <b>ARCHITECT</b> <b>GENERAL CONTRACTOR</b> <b>PROJECT MANAGER Ed Debroeck KPFF</b>
<b>FROM</b> Pacific Pile & Marine-Nicole Egli  	

#### QUESTION

Concrete delamination has been observed between the ore loader platform concrete support beams and the existing concrete structure during the wire sawing operation (photos attached).

The delamination may cause the concrete to separate from the support structure after the completion of the wire sawing or while picking the associated concrete section.

In addition to the safety concerns of an uncontrolled separation, the timing of the separation may also damage underneath equipment (if occurring during structure removal) or may damage adjacent wire sawing equipment. Concrete beam sections that fall in the water may also be in conflict with future pile installation and require further removal by divers.

To mitigate the safety and equipment concerns, and conflicts to future work, PPM will install support for the beams to prevent separation from the structure.

The existing concrete structure and support beams will be investigated for potential differing site conditions from what was presented in the Appendix D Historical Drawings and Reports once they have been removed and can be safely inspected.

PPM will track any additional costs or schedule impacts incurred for providing additional support to the beams and forward tracking documents to the Owner's representative for hours verification on a daily basis.

Please confirm that the proposed measures are acceptable to KPFF/MOS or provide additional direction.

#### REFERENCES/ATTACHMENTS

SPECIFICATIONS

DRAWINGS

OTHER  
True

#### PROPOSED SOLUTION

POTENTIAL COST IMPACT  
Yes

POTENTIAL SCHEDULE IMPACT  
Yes

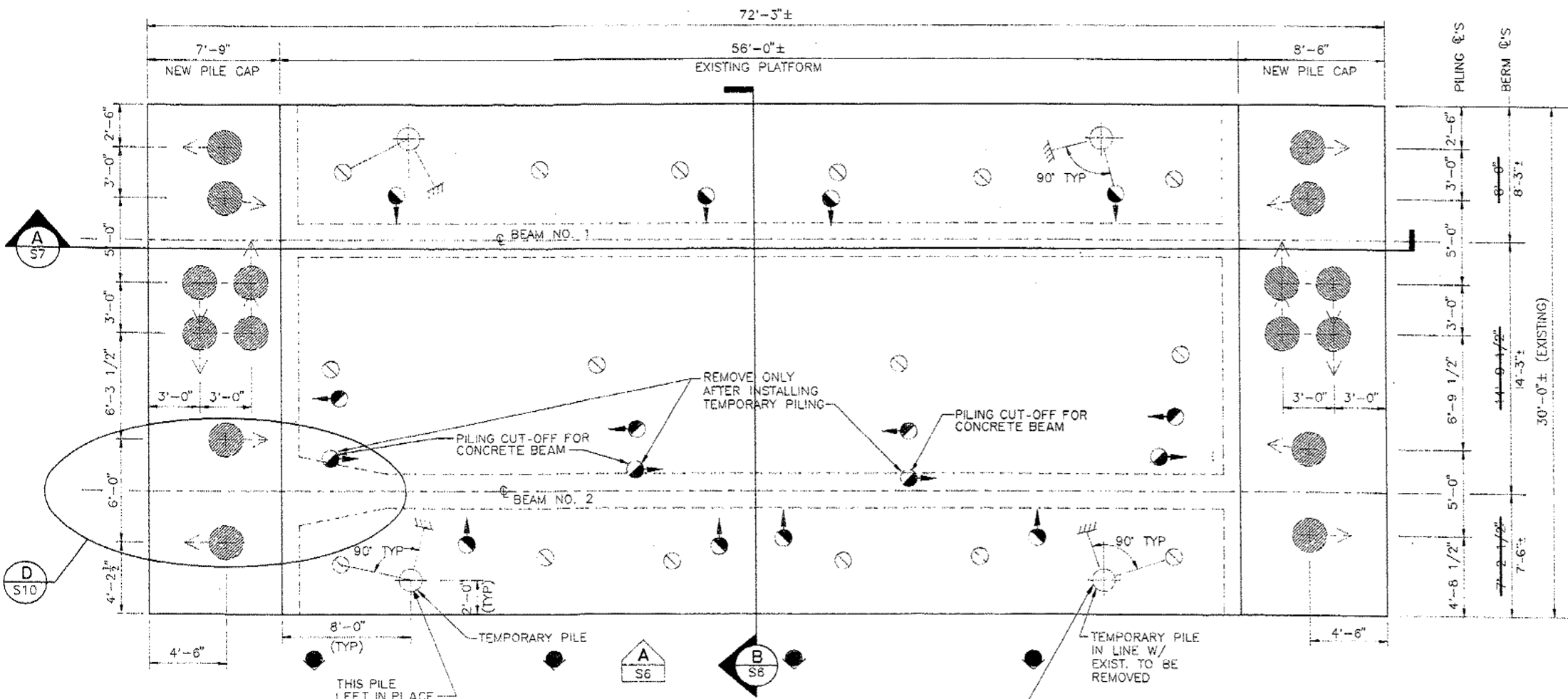
RESPONSE

As discussed in meeting on 11/22 PPM's engineer shall submit a stamped drawing and calculation package for the added supports to hold the beams. PPM's engineer would also need to submit calculations showing how they intended the beam to be self supporting and what is different from that plan in the existing conditions.

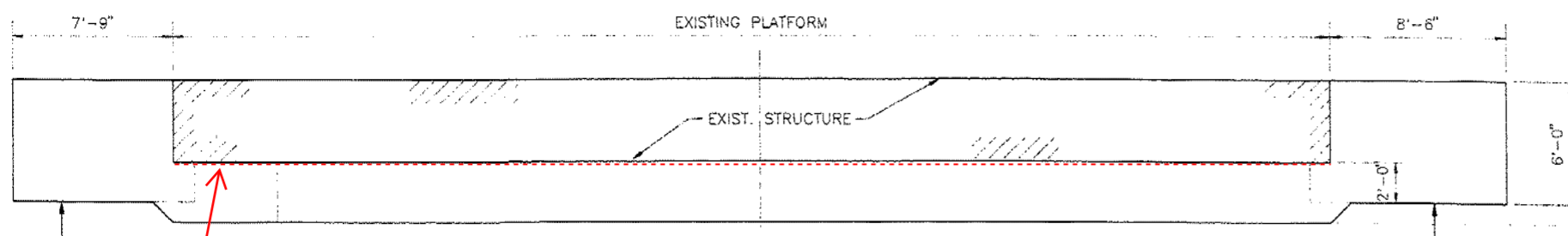
As discussed on 11/28: PPM's Engineer shall also consider the post tensioning forces that may be pulling the end of the beam down away from the cap as it is cut.

As discussed 3/2024 PPM to demonstrate/document locations of #7 bars holding soffit beam to onsite inspector during demolition

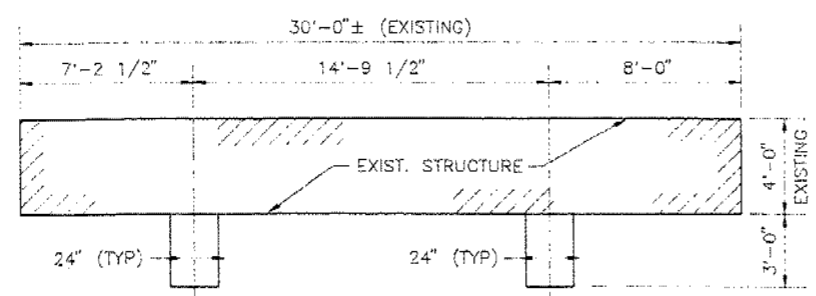
RESPONSE BY  
Ed DeBroeck 11/29/2023



**S5** SHIP LOADER PLATFORM PLAN  
SCALE: 1/4" = 1'-0"



**S6** SHIP LOADER PLATFORM ELEVATION  
SCALE: 1/4" = 1'-0"



**S6** SHIP LOADER PLATFORM SECTION  
SCALE: 1/4" = 1'-0"

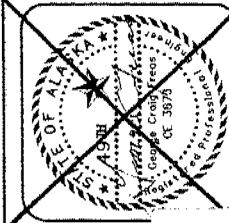
RFI 102 - Observed delamination between beams and existing structure

**NOTES**

1. BATTER NORTH-SOUTH PILING 3V:1H, AND EAST-WEST PILING 4V:1H.
2. BULL RAIL ON NEW CAPS NOT SHOWN.
3. LOADER TOWER AND OTHER FEATURES ON EXIST. PLATFORM NOT SHOWN.
4. TEMPORARY PILING:
  - TEMPORARY PILING ARE TO BE INSTALLED ONLY IF IT IS NECESSARY TO REMOVE EXISTING PILING IN ORDER TO CONSTRUCT NEW BEAMS UNDER THE SHIPLOADER PLATFORM.
  - FOR EACH PILE REMOVED PROVIDE ONE 18" DIAMETER X 0.375" WALL TEMPORARY PILE.
  - DRIVE TEMPORARY PILES TO A 30 TON WORKING CAPACITY, BUT NO LESS THAN 50 FT. PENETRATION.
  - CUT OFF TEMPORARY PILING AT AN ELEVATION SUFFICIENT TO ALLOW INSTALLATION OF 30 TON JACKS.
  - BRACE THE TOP OF TEMPORARY PILING AGAINST ADJACENT PILING OR THE UNDERSIDE OF THE LOADER PLATFORM WITH A MINIMUM OF TWO BRACES POSITIONED AT APPROXIMATELY 90° TO ONE ANOTHER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DESIGNING THE BRACES AND PROVIDING AN INSTALLATION PLAN TO THE ENGINEER.
  - WELD A MINIMUM 1" THICK PLATE TO THE TOP OF THE TEMPORARY PILING AND PLACE A BEARING PLATE BETWEEN THE 30 TON JACKS AND THE LOADER PLATFORM. THE BEARING PLATE SHALL BE 1" THICK AND HAVE A BEARING AREA OF NOT LESS THAN 1" SQUARE FOOT.
  - LOAD AND MAINTAIN EACH OF THE JACKS AT 20 TONS. LOAD THE JACKS SEQUENTIALLY IN 4 TON INCREMENTS.
  - THE CONTRACTOR MAY INSTALL ADDITIONAL TEMPORARY PILING.
  - REMOVE TEMPORARY PILING AND BRACING FOLLOWING POST-TENSIONING OF BEAMS NOL. 1 AND NO. 2.
  - TEMPORARY PILING NEED NOT BE GALVANIZED.
5. DO NOT REMOVE FORMS OR SHORING FOR BEAMS NO. 1 AND NO. 2 UNTIL CONCRETE REACHES SUFFICIENT STRENGTH TO TRANSFER WEIGHT OF BEAMS TO #7 DOWELS IN VENT HOLES.

DESIGN	GCF
DRAWN	NRY
CHECK	DHA
APPROVED	GCF

**R&M ENGINEERING, INC.**  
 8205 GLACIER HIGHWAY  
 P.O. BOX 34278  
 JUNEAU, ALASKA 99803  
 PH: (907) 780-8080



SOT03-1991-AB-S6, Loader Platform - Plan & Sections

ALASKA INDUSTRIAL DEVELOPMENT & EXPORT AUTHORITY  
 SKAGWAY ORE TERMINAL STRUCTURAL REPAIRS  
 SKAGWAY, ALASKA

**LOADER PLATFORM PLAN & SECTIONS**



**AS-BUILT**



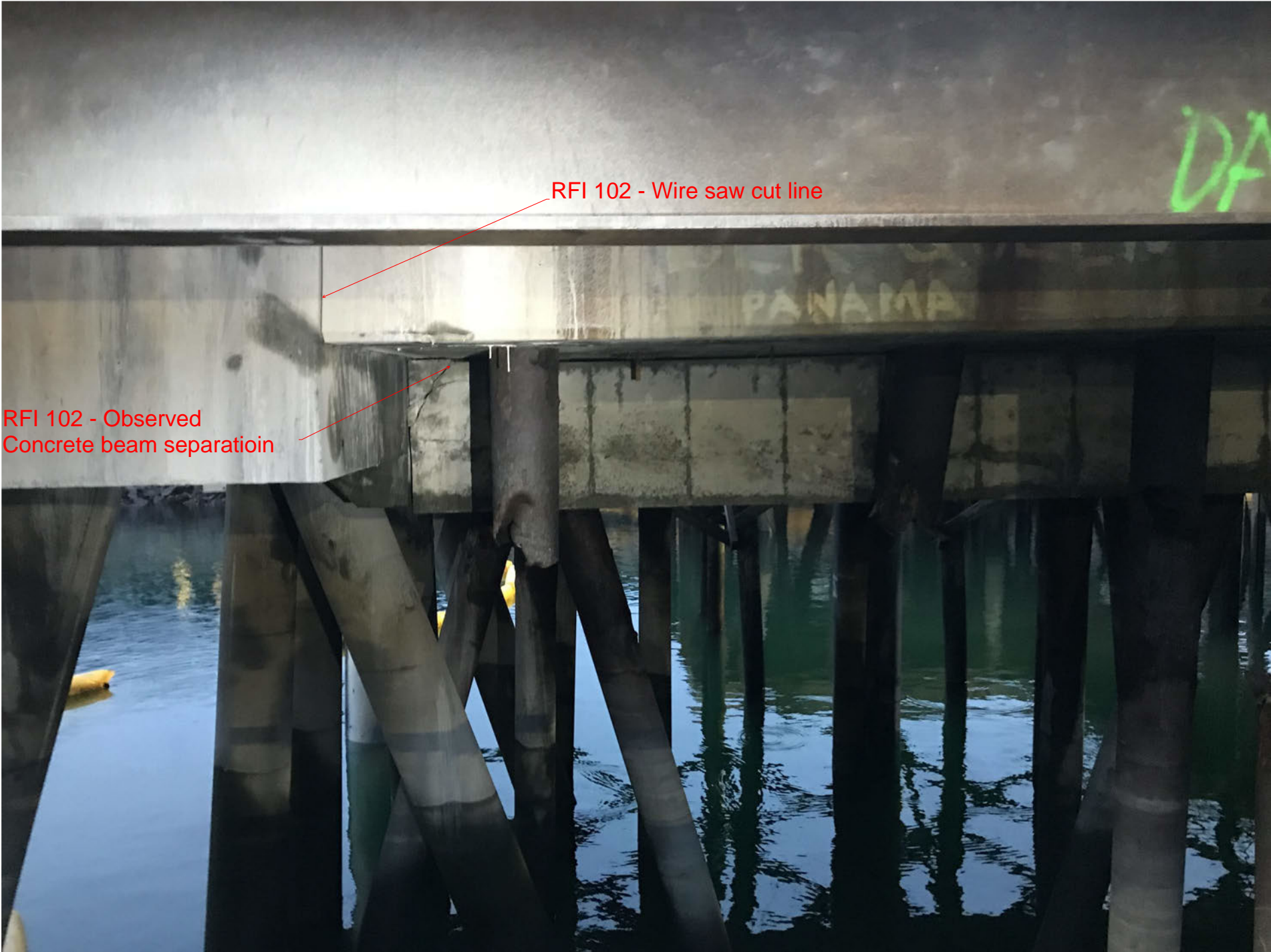
DATE: DEC. 6, 1991  
 R&M NO. 901362.03  
 SCALE: AS NOTED

**S6**

SHEET  
 6 of 18

E: 05189-S6, 1=48, 12/06/91 at 14:35





RFI 102 - Wire saw cut line

RFI 102 - Observed  
Concrete beam separation

Pacific Pile and Marine  
700 S. Riverside Drive  
Seattle, Washington  
98108

December 11, 2023

Attn: Nicole Egli

Nicole,

We received your email dated 12/9/2023 concerning the delamination of the precast structural segments from the bottom of the Ore Loader Platform during the saw cutting process.

In regards to that we can offer the following:

The plan call for #7 bars grouted in 2" drilled holes spaced 8 ft o.c. along the CL of the Beams. The as Built's show a count of 6 ea. which coincides with the 8 ft spacing.

At this spacing, there will be 2 ea. dowels per sawcut segment, however, the dowels in the first and last segments will not be centered on the C.G. of the precast mass, so that needed to be taken into account during the analysis.

We considered the following possible failure mechanisms:

- Steel tension failure
- Grout bond to concrete failure
- Grout shear failure
- Bar development length Failure
- Concrete shear cone failure

The driving element in this check was the bond failure between the grout and the concrete slab. ACI has two recommendations for the bond stress in such elements. The first is 200 psi based on a cracked condition. The second is 650 psi based on an uncracked condition. Both of these stresses pertain to an outdoor condition. Technically, the concrete slab is not in a cracked condition and the 650 psi would apply. We performed two checks. The first was using an average of cracked and uncracked to account for possible anomalies given the age of the structure. This produced an overall factor of safety of 6:1 on the bond and drove the limiting fact of safety to the grout shear strength which was 1.78: 1. The second check we ran was the

cracked condition. This produced an overall factor of safety of 1.5:1. All of which would be considered acceptable for demolition operations such as this.

Had the #7 bars been compromised, you would have seen either delamination between the posttensioned beam and the non precast slab or you would have seen rust stains running down the posttensioned every 8 ft on center. Without one of those two indications, there would be no reason to suspect that the bars were compromised.

There was a comment in you email concerning cutting post tensioned beams and there being forces that will pull the beam down and away. Cutting a fully grouted post tensioned beam such as this will simply produce shorter post tensioned beams. There will be no downforce exerted by the beam on the concrete due to the cutting operation other than the fact that the slab itself would deflect downwards as the beam supports are essentially removed.

We can't offer any opinion as to why the beam is delaminating from the slab as we have not inspected the area or the elements that are delaminating. Given the as built condition shown on the plans that we received, we can see no reason as to why the beams should have delaminated from the slab if the coring and grouting procedures were performed correctly at the time of initial construction.

If you have any questions regarding this letter, please call me at (707)-333-4977

Sincerely,

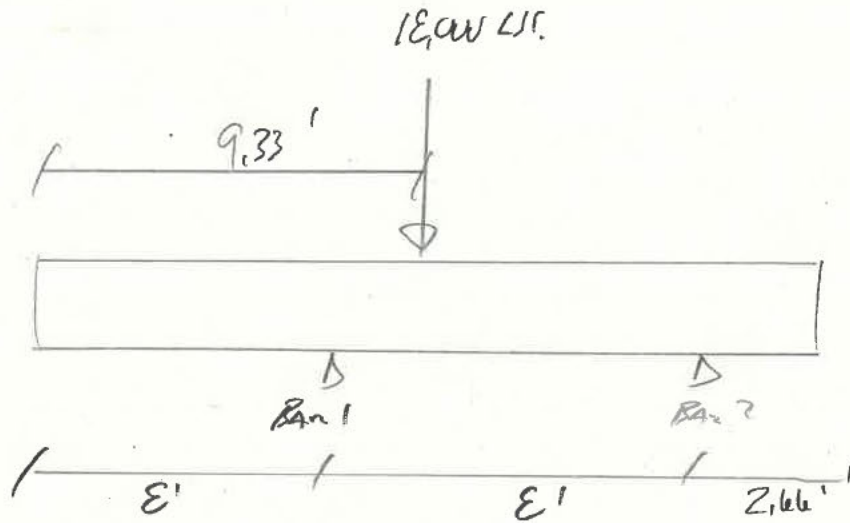


Brian Mapel, P.E., P.Eng  
Principal/Owner  
BMA Construction Engineers Inc.



12/31/23

Worst Case Piece



$$\frac{18,000 \times 6.667}{8} = 15,007.5 \text{ LBS.}$$

Max BAR load = 15,007 LBS.

F.S. steel =  $\frac{21.55}{15} \times 1.67 = 2.4 : 1$

F.S. Grout Bond =  $\frac{48025}{15,000} \left(\frac{1.4}{1.7}\right) = 2 : 1$

F.S. Conc shear =  $\frac{14,325}{15,000} \times \frac{1.4}{.75} = 1.78 : 1$

F.S. Conc core = OK

F.S. Grout Bond in Cracked Concrete =  $\frac{(200)(226)(.70)}{1.4(15,000)} = 1.5 : 1$

## Check Steel

$$18.66 \times 2 \times 3 \times 160 \text{pcf} = 17,913 \text{ Lbs concrete load.}$$

Tensile Capacity of #7 bar:

$$A_{\text{bar}} = .60 \text{ in}^2$$

$$F_y = 60 \text{ ksi}$$

$$F_u = 80 \text{ ksi}$$

Tensile Yield:

$$\frac{F_y A_s}{\Omega} = \frac{60 (.60)}{1.67} = 21.55 \text{ kips}$$

Tensile Rupture:  $\frac{F_u A_e}{\Omega} = \frac{80 (.60)}{2} = 24 \text{ kips.}$

#7 BAR Steel Capable of Supporting 21.55 kips

$$\text{TOTAL Dead Weight} = 17,913 \text{ kips}$$

$$\text{TOTAL Dead Capacity} = 43.10 \quad \underline{\underline{\text{OK}}}$$

$$\text{FS} = \frac{43.10}{17,913} \times 1.67 = 4 : 1$$

## Check Grout Bond

425 represents the Avg of  
CRACED (uneven) concrete. The concrete  
is increased by definition  
but Average allows for some  
imperfection

$$\text{Bond Strength} = 425 \text{ psi}$$

ACI Table 17.4.5.2  
(Supplement 1988) cement/sand grout

$$\text{Bond Area} = 2" \times \pi \times 36 = 226$$

$$\phi = .70$$

ACI

$$\text{Bond Capacity} = (425)(226) \\ = 96,050$$

$$\text{Load Factor} = 1.4$$

$$\text{Design Capacity} = \frac{96,050 (.7)}{1.4} = 48,025 \text{ LBS.}$$

$$2ea = 96,050 \text{ LBS.}$$

$$\text{Actual load} = 17,913 \text{ LBS.}$$

OK

$$F.S. = \frac{96,050}{17,913} \left( \frac{1.4}{.7} \right) = 11 : 1$$

## Check Concrete Shear at Bond Interface

$$V_c = 2 \sqrt{f_c'} A_{con} \quad \text{ACI}$$

$$A_{con} = \pi (2)(36) = 226 \text{ in}^2$$

$$f_c' = 3500 \text{ psi}$$

$$\text{Shear Strength} = 26,740 \text{ LBS.}$$

$$\text{Design Capacity} = \frac{26,740 (.75)}{1.4} = 14,325 \text{ LBS.}$$

$$14,325 \times 2 = 28,650 \text{ LBS.} \quad \text{OK}$$

$$F.S. = \frac{28,650}{17,913} \left( \frac{1.4}{.75} \right) = 3 : 1$$

## Check Development Length

$$L_d = \frac{f_y \psi_t \psi_c}{20 \lambda \sqrt{f_c'}} d_b$$

$$f_y = 60,000 \text{ psi}$$

$$d_b = .875$$

$$f_c' = 3,500 \text{ psi}$$

$$\psi_c = 1.0$$

$$\psi_t = 1.0$$

$$= 44.370$$

Actual  $L_d = 36 \text{ in}$

BAR develops  $\frac{36}{44.37} = 82\%$  of strength

$$43.10 \text{ kips (.82)} = 35.34 > 17.913 \quad \underline{\text{OK}}$$

## Check Concrete Core Breakout

$$\phi = .70$$

$$A_{nc} = 9 \text{ hef}^2 = 9 (36)^2 = 11,664 \text{ in}^2$$

$$A_{nc} = \begin{array}{c} \square \\ \text{96"} \\ \text{96"} \end{array} = 9,216 \text{ in}^2$$

$$\psi_{ed,N} = 1.0$$

$$N_b = k_c \lambda \sqrt{f_c'} \text{ hef}^{1.5}$$

$$\psi_{c,N} = \frac{1.4}{1.0} \quad \begin{array}{l} \text{(cur. transverse)} \\ \text{(cracking)} \end{array}$$

$$= 17(1) \sqrt{3500} (36)^{1.5}$$

$$= 217,238$$

$$\psi_{cp,N} = 1.0$$

$$N_{cb} = \frac{A_{nc}}{A_{nc}} \psi_{ed,N} \psi_{c,N} \psi_{cp,N} N_b$$

$$N_{cb} = \frac{9216}{11,664} (1) (1) (1) 217,238$$

$$= 171,645$$

$$\phi = .7$$

$$\phi N_{cb} = 120,000 \text{ lbs} > 17,200 \text{ lbs}$$

$$\quad \quad \quad \times 2$$

$$\quad \quad \quad = 240,000 \text{ lbs}$$

OK



# Skagway Ore Terminal Ship Loader Platform Demolition Post Tension Beam support system Calculations



12/31/23

Brian Mapel, P.E., P.Eng  
BMA Construction Engineers Inc.  
For: Pacific Pile  
12/13/2023



### Weight of Post Tensioned Beam:

$$18.67' * 2' * 3' * 160 \text{pcf} = 17,923 \text{ LBS.}$$
$$= 18,000 \text{ LBS.}$$

### CAPACITY OF HANSEN ASSEMBLY

Tension Capacity of 1 1/4" F1554 Grade 105 rebar

$$\text{Allowable Tension} = 43.1 \text{ ksi}$$

$$\text{Area Bar} = \frac{(1.25)^2 \pi}{4} = 1.227 \text{ in}^2$$

$$\text{Tensile Capacity} = (1.227)(43.1) = 53,25 \text{ Kips} \quad \underline{\underline{OK}}$$

[ Per S10.5.5.4.1 One Terminal  
Shrinkage Reduction (ALS Rev 1  
DATED 8/25/23 Known here AFM  
at Ref (1) ]

### Check Bearings plate Bearings on Concrete

$$\text{Allowable Bearing Stress} = 4920 \text{ psi per Ref (1)}$$

$$\text{Red Load} = 18,000 / 4 = 4,500 \text{ LBS} * 1.6 \text{ (Load fact.)} = 7,200 \text{ LBS}$$

$$7 * 7 * 4920 = 216 \text{ Kips Concrete Capacity} \quad \underline{\underline{OK}}$$

### Check Bearings Plate Distribution Ability

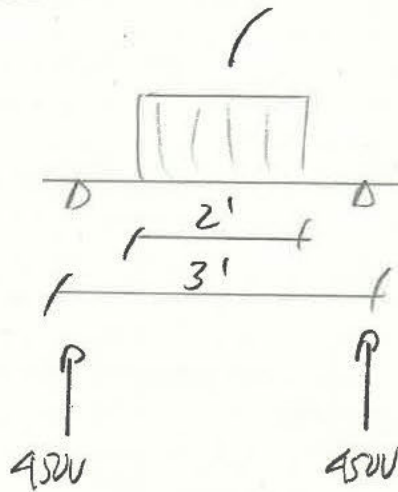
$$\text{Plate Capacity is } 26 \text{ Kips per Ref (1)} \quad \underline{\underline{OK}}$$

### Check Plate Bearings on HSS Tube

$$\text{Capacity is } 156 \text{ Kips per Ref (1)} \quad \underline{\underline{OK}}$$

Check HSS Tube

$$q_{wall} = 4500 \text{ LBS/FT}$$



Max Tube Bending:

$$= 4500 (1.5) - (4500)(1)\left(\frac{1}{2}\right)$$

$$= 4500 \text{ ft-lbs}$$

$$= 54000 \text{ in-lbs}$$

HSS 7x5x1/2

Laying FLAT

Check FLANGE Compactness:

Weak orient.

$$b = 5.5$$

$$t = .5$$

$$b/t = 11$$

$$11.2 \sqrt{E/F_y} = 27$$

Check Web Tube Orientation

$$b = 3.5$$

$$t = .5$$

$$b/t = 7$$

$$2.42 \sqrt{E/F_y} = 58$$

Compact in Any Orientation

Yielding:  $M_n = M_p = F_y Z$   
 $= (50)(14.2)$  — use 5 in stead of 2 for  
Addition safety  
 $= 710 \text{ in-lb}$       OK

Call Before you dig numbers:

U/G Water, Sewer : 907-983-2449

U/G Power, Telephone, TV: 907-983-2202

**General Notes:**

1. All welding done in accordance with AWS D1.1 use E70XX electrode
2. All Beams, angles, and plates to be A36, Fy=36 ksi min.
3. Piles to be A53 Gr. B, Fy=35 ksi min.
4. Tube Steel to be A500 Gr. C, 50 ksi min.
5. Existing Concrete assumed  $f'_c=4,000$  psi

Maximum anticipated deflection including second order effects when lateral load occurs is 2.5"

**Design Codes:**

1. ASCE 7-16 with construction loading recommendations from ASCE 37-14
2. AISC Steel Construction Manual 15th Edition
3. ASME BTH-1-2017. Class A Lifter.  
During the picking of the sections from the falsework system, all elements that were designed as falsework that are integral to the pick were then designed using ASME standards.
4. All manufactured rigging components to have a factor of safety of 5.0 min.

**Design Loads:**

Analysis done per ASD procedure accounting for Live Load, Dead Load, lateral load, water load, and wind load

Live Load: 20 psf on concrete structure

Dead Load: Concrete unit weight 160 pcf

Steel unit weight 490 pcf

5 ft of all piles supporting the system are assumed to remain on the segments as they are lifted.

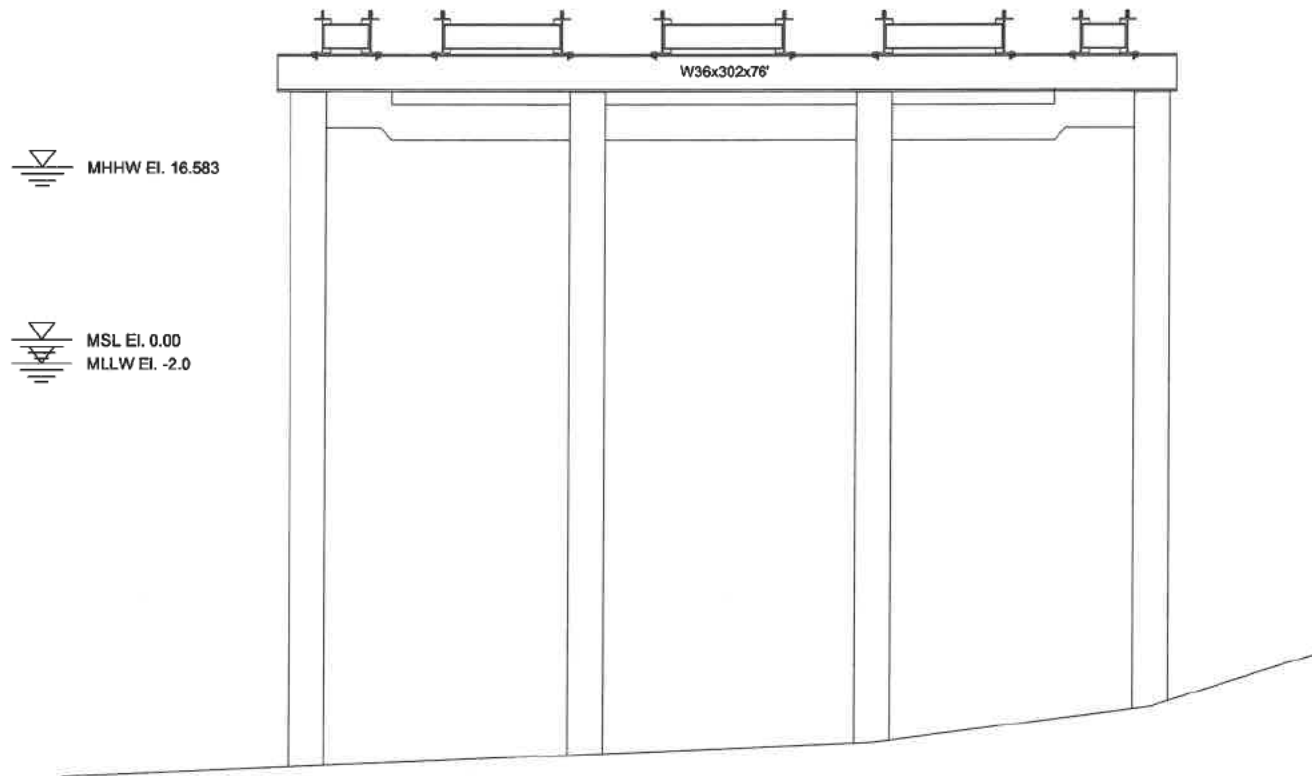
**Lateral Load:**

Water load from a 1.5 knot current

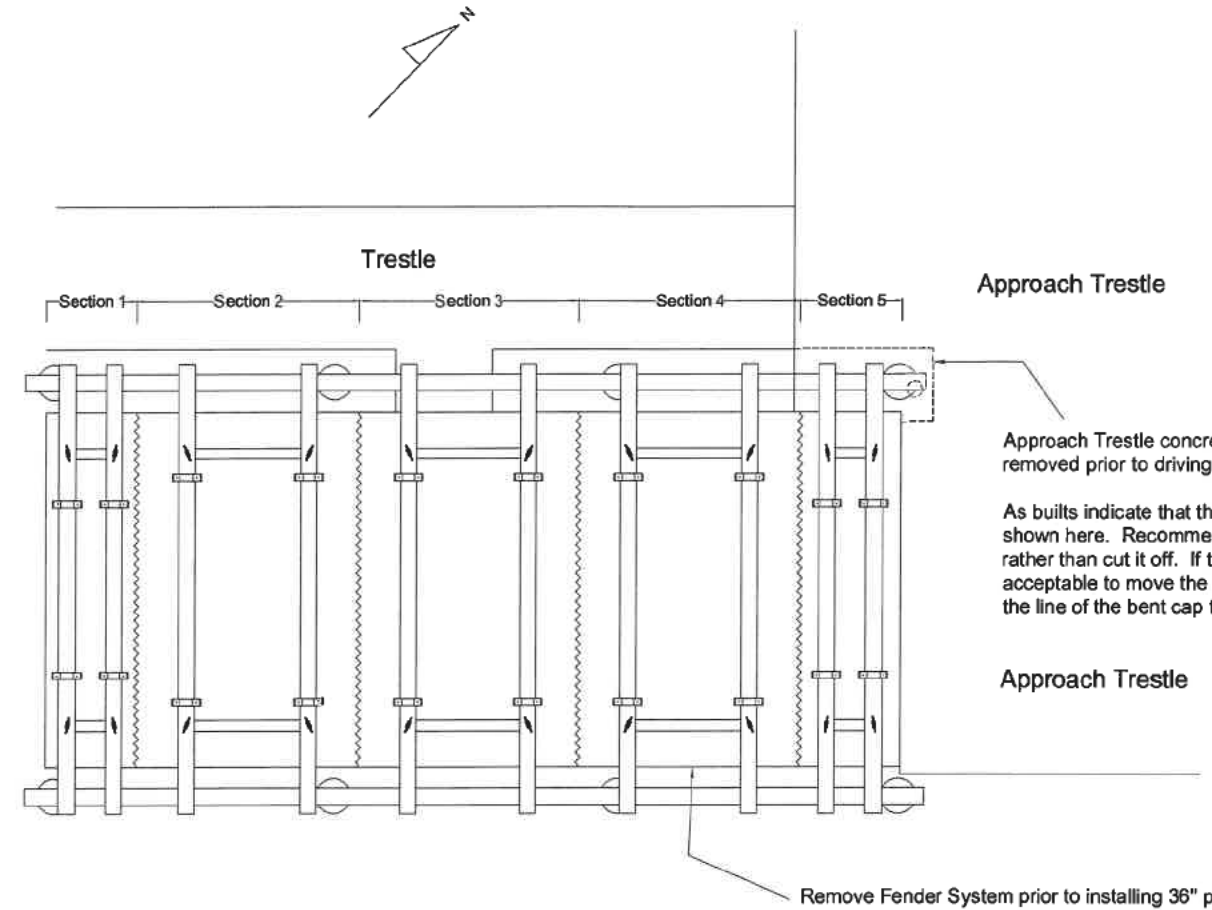
Wind Load using ASCE 7-16/37-14 Exposure D, Risk Category I

2% of Dead Load of Structure

**Pick Parameters:** Crane will be sized to have sufficient capacity to lift the segments shown at the weights indicated. Wind load has not been applied to the picked element. For wind speeds less than 20 mph, horizontal wind forces on the load will less than 1,000 lbs.



Longitudinal Cross Section



Plan



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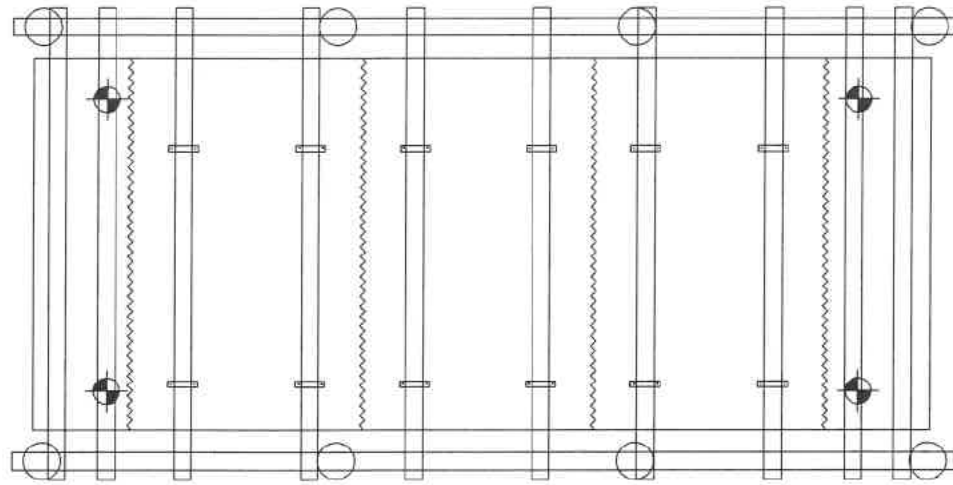
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1515 Oakland Blvd. Suite 220  
Walnut Creek, Ca 94596  
www.thebmacorp.com

**Pacific Pile**

Skagway Ore Terminal Demolition

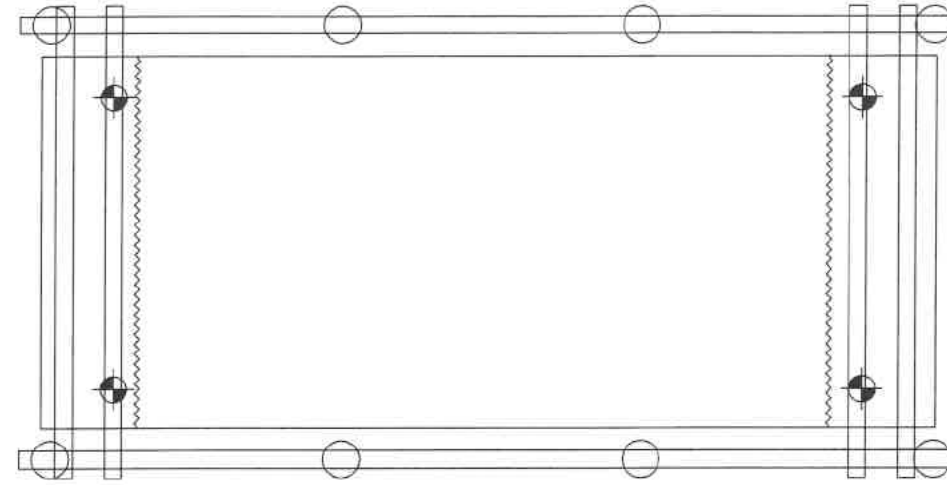
General Plan

Sheet 1 of 16



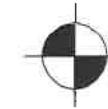
Stage 1:

1. Drive pile, weld pile caps, install cap beams.
2. Set stringers over structure and tie together for future rigging operation per these plans.
3. Core holes for bolts and install bolt supports.
4. Attach stringers to concrete where indicated to act as bracing for the platform.
5. Tension Bolts and Wire cut concrete along lines shown.
6. Cut pile beneath middle sections of concrete slab. Leave large pile under end sections in tact.

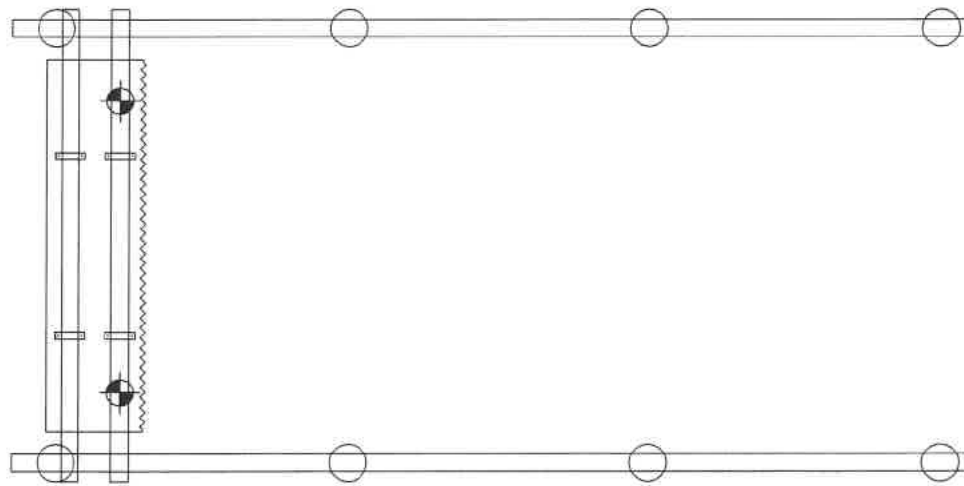


Stage 2:

1. Remove Middle Sections.

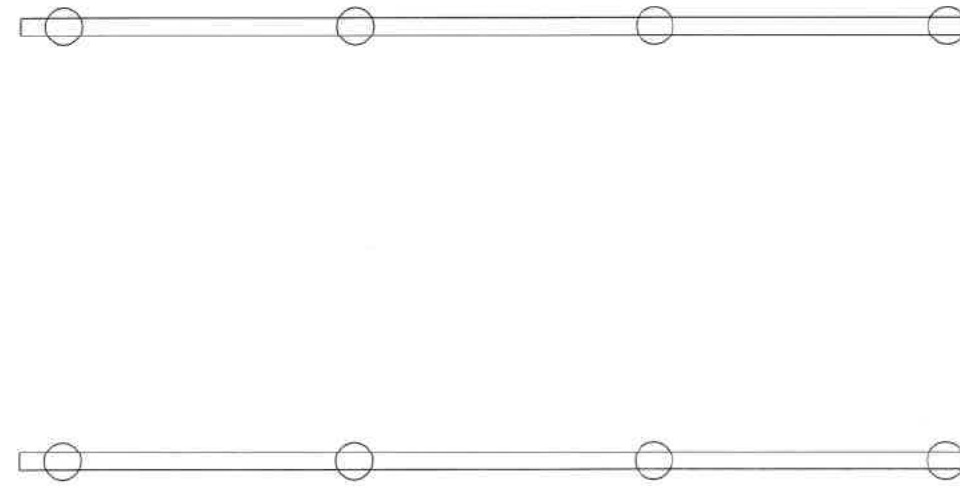


Indicates beams designated as Anchor Beams. See Anchor Beam Detail Sheet for Connections



Stage 3:


1. Cut Pile under Right End Block and remove end block

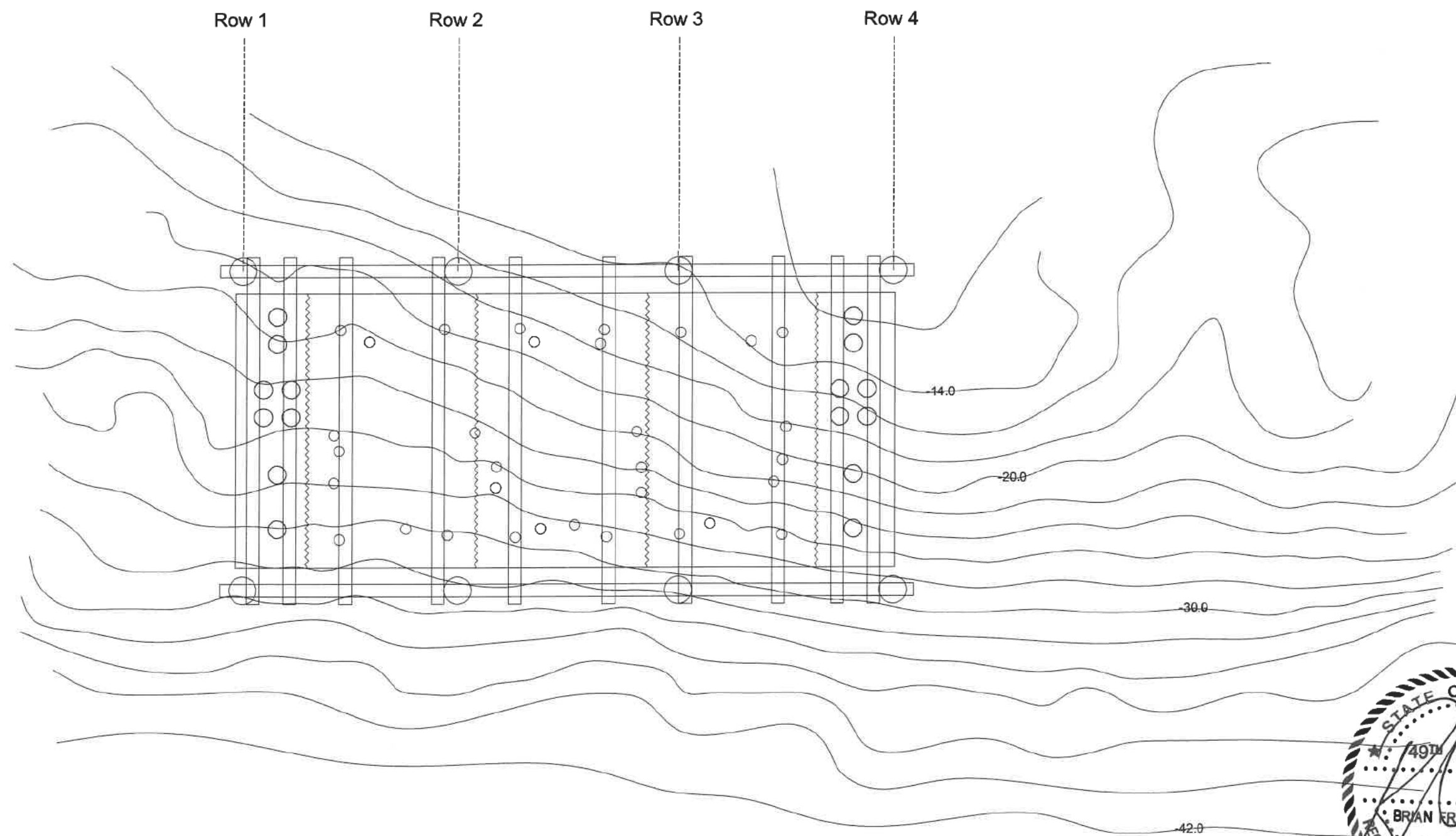


Stage 4:

1. Cut Pile under Left End Block and remove end block



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**PACIFIC PILE**

Skagway Ore Terminal Demolition  
Topography

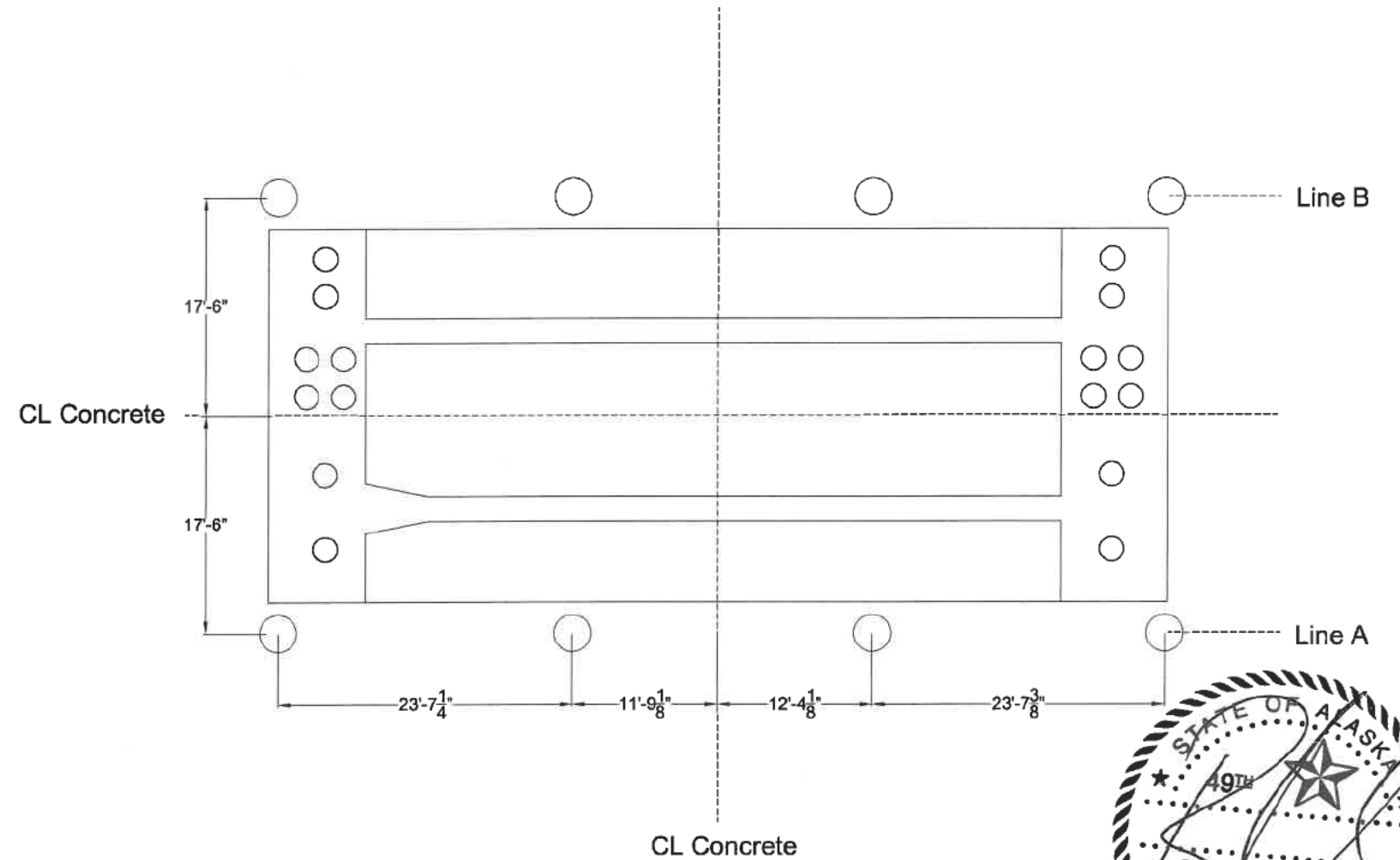
Sheet 3 of 16

Pile	Axial Load kips	Mom. Long ft-kips	Mom. Trans ft-kips	Min. Embed ft	Estimated Embed ft	Depth to fixity ft
1a	138	101	47	20	48	9
1b	138	155	30	25	48	9
2a	325	110	47	20	75	9
2b	325	185	34	25	75	9
3a	320	110	50	22	75	9
3b	320	205	40	24	75	9
4a	140	215	50	26	48	11
4b	140	375	40	31	48	12

Axial, Long moment, and transverse moment do not necessarily occur at the same time. See structural calculations for combined loading.

**Determining Pile Capacity in the Field:**

1. If using manufacturers hammer charts the charts must be based on the modified gates formula, not the ENR formula. There should be no factor of safety incorporated in the chart. The chart should publish ultimate capacities. Take the axial load in the above chart, multiply them by 3.0, and find that number on the manufacturers chart.
2. If using a PDA with CAPWAP, then use a factor of safety of 2.0



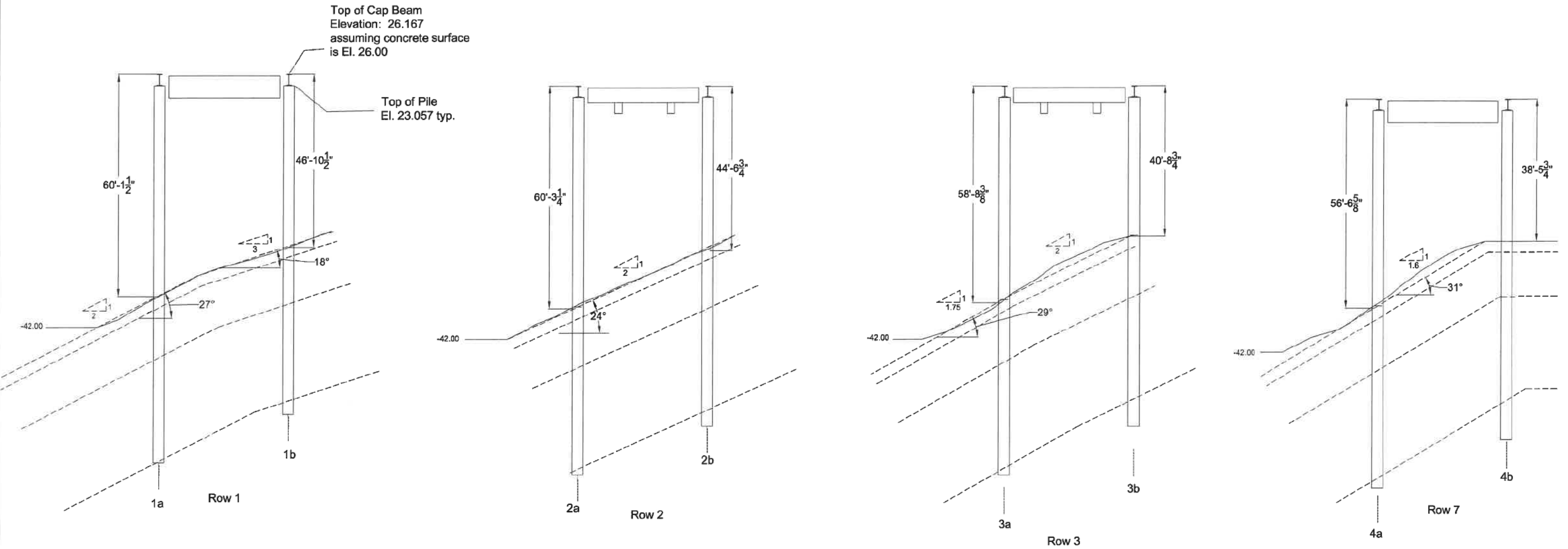
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**PACIFIC PILE**

Skagway Ore Terminal Demolition  
 Pile Layout and Data



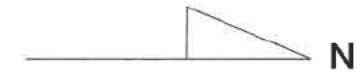
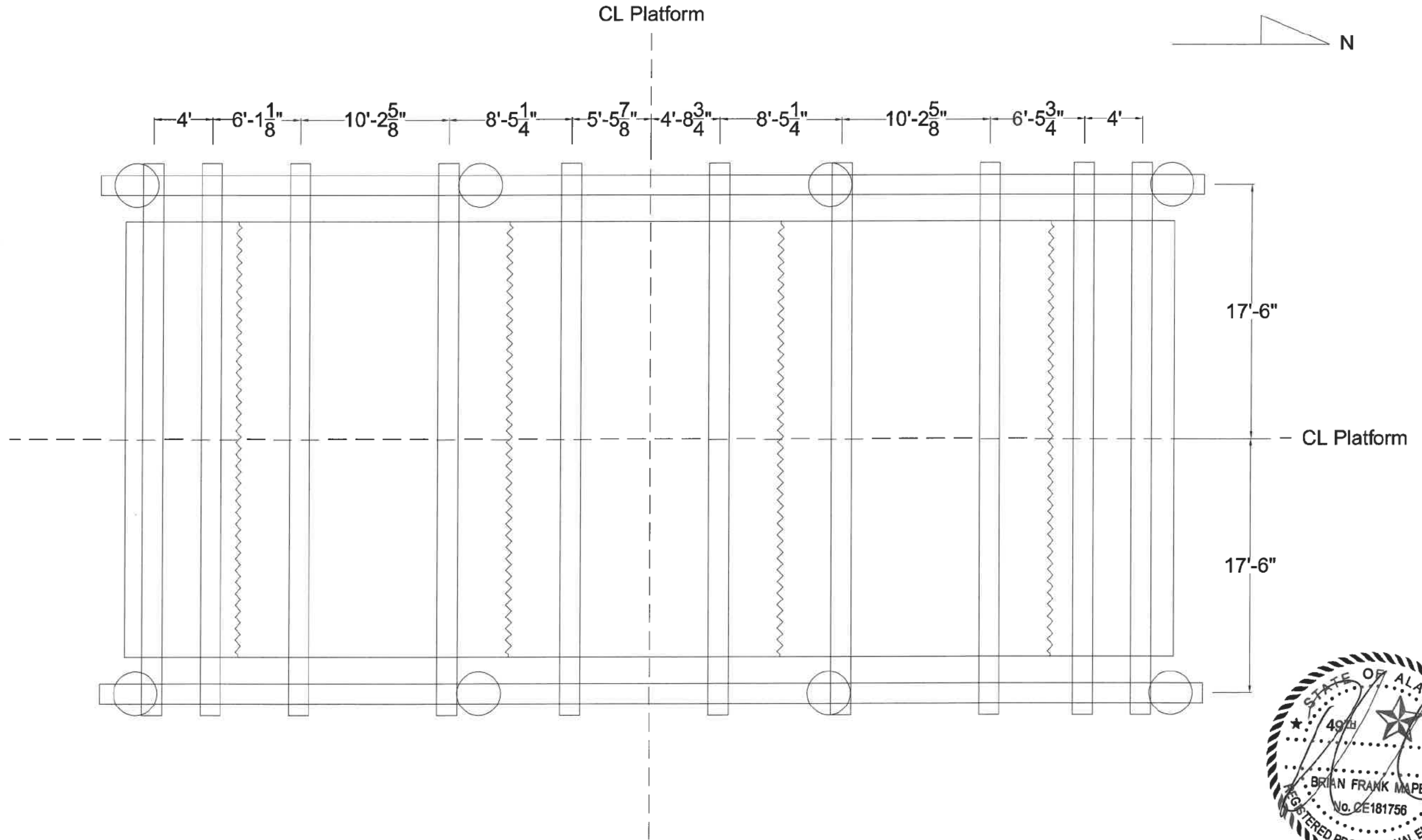


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**PACIFIC PILE**

Skagway Ore Terminal Demolition  
 Row Profiles



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**Pacific Pile**

Skagway Ore Terminal Demolition

Beam Layout

C Clamp. Torque to 105 ft-lbs, typ.

CL Concrete

CL Bracing/Padeye

W36x302x38'

Elevation View

L5x5x3/4" Angle, typ.  
See Details Sheet for angle iron to Concrete Detail, typ.

1-1/4" Ø F1554 Grade 105  
OR A193 Grade B7  
Threaded Rod with Nut  
and Washer, typ.  
Torque to 1100 ft-lbs.

L5x5x3/4" Angle, typ.  
See Details Sheet for beam to angle detail.

See Bracing Details sheet

L5x5x3/4" Angle, typ.  
See Details Sheet for angle iron to Concrete Detail, typ.

See Bracing Details sheet

C-Clamp Locations

C Clamp. Torque to 105 ft-lbs, typ.

4 1/2" typ.

Core 2" Hole in concrete slab for threaded rods, typ.

Transverse Cross Section

2'-5" long HSS 7x5x1/2"  
Drill 1-1/8" hole for threaded rod, typ.

2 ea. 7x7x1-1/4" thick plate, typ.  
Drill 1-3/8" hole in center for threaded rod.

5/16" 4" long both side of HSS, typ.

A36 1-1/4" thick 7x7 base plate, typ.  
Drill 1-3/8" hole for threaded rod.

Plan View



12/31/23

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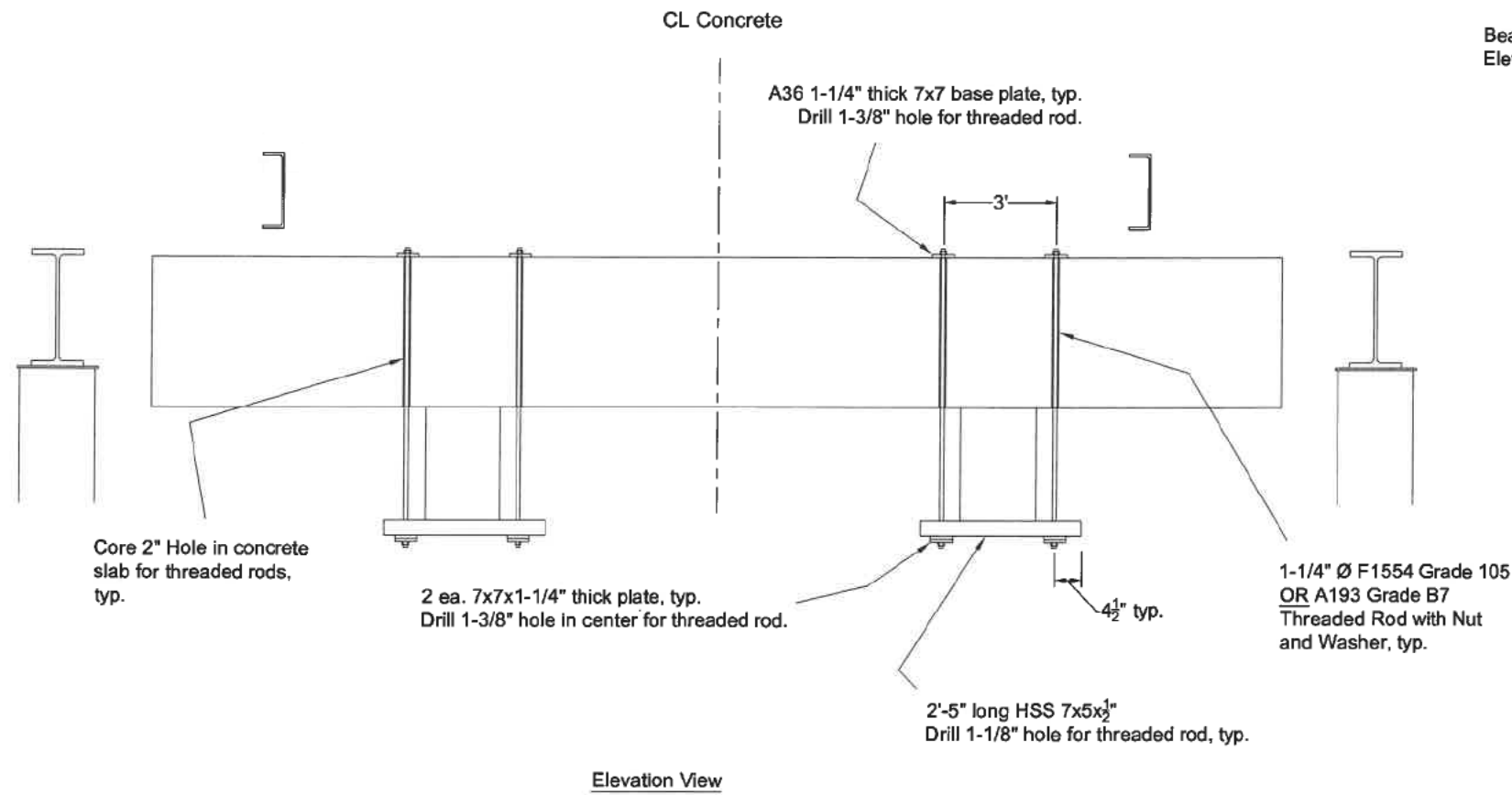
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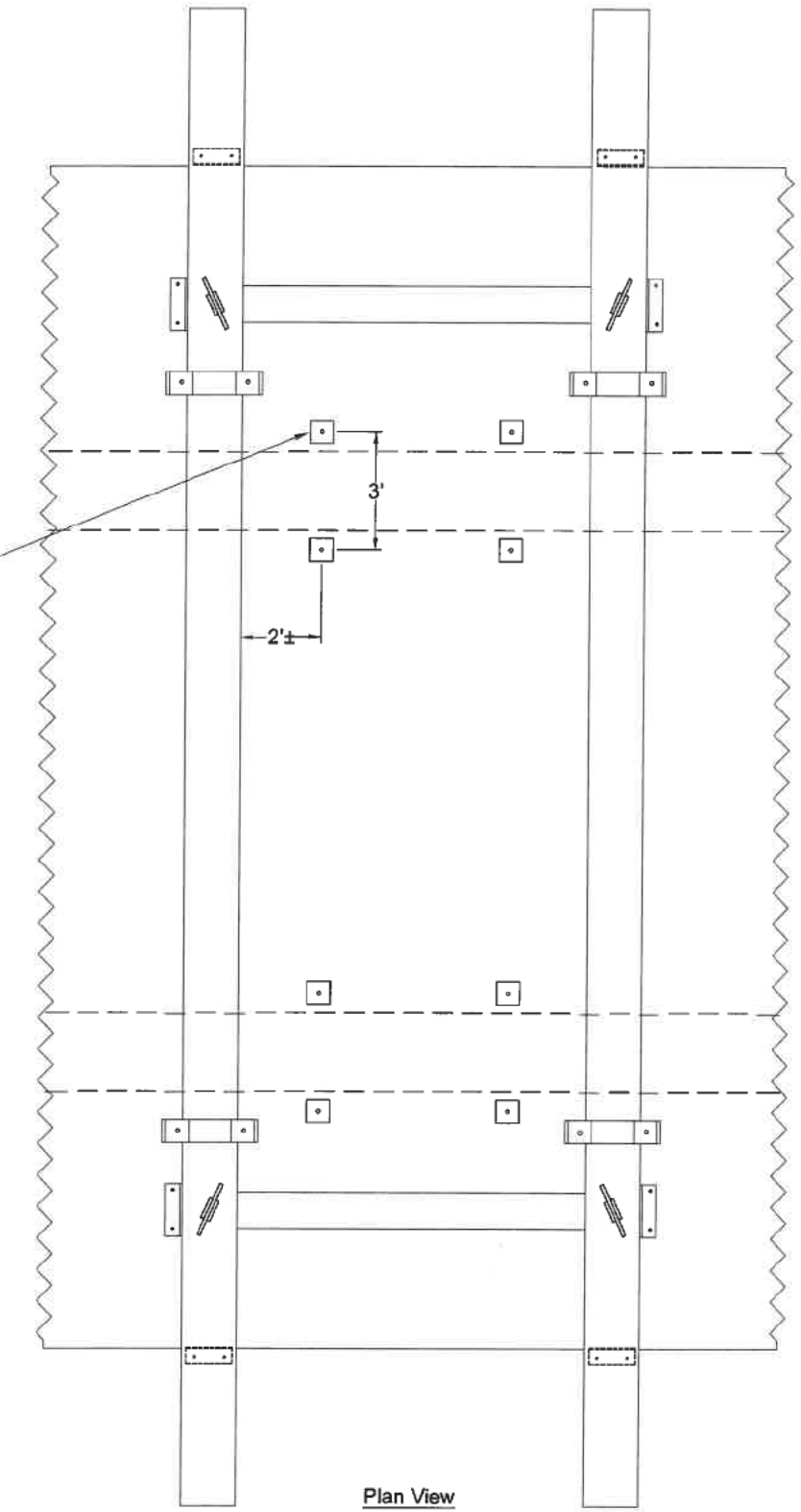
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Section 2-4 Falsework Details

Sheet 7 of 16



Beam Supports. See Elevation View for details



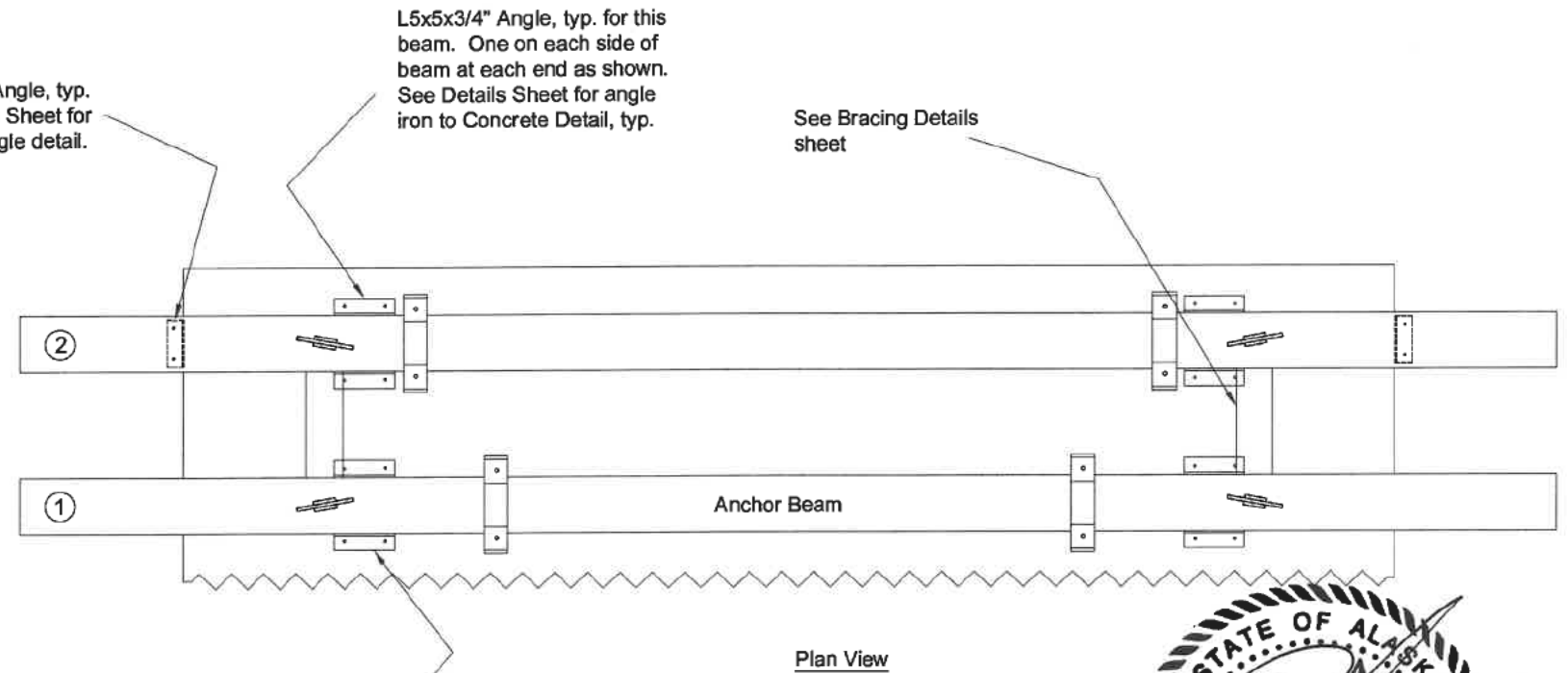
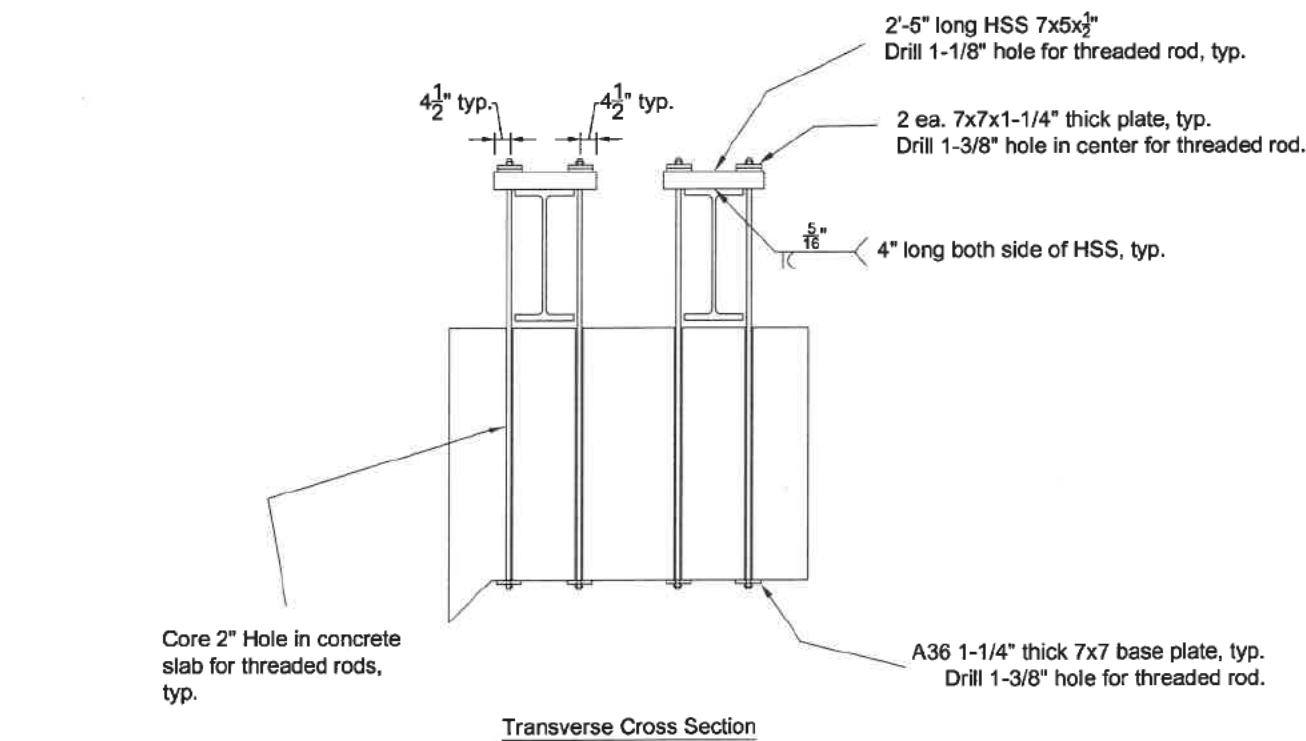
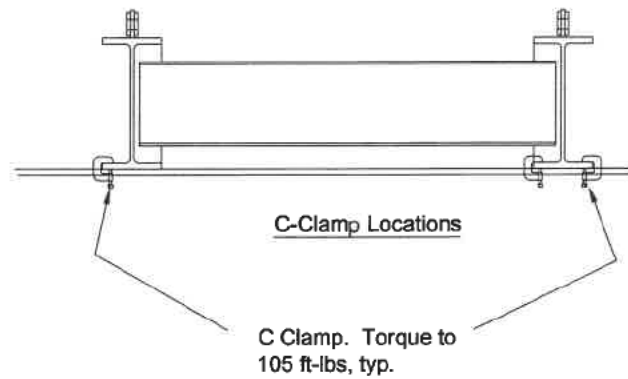
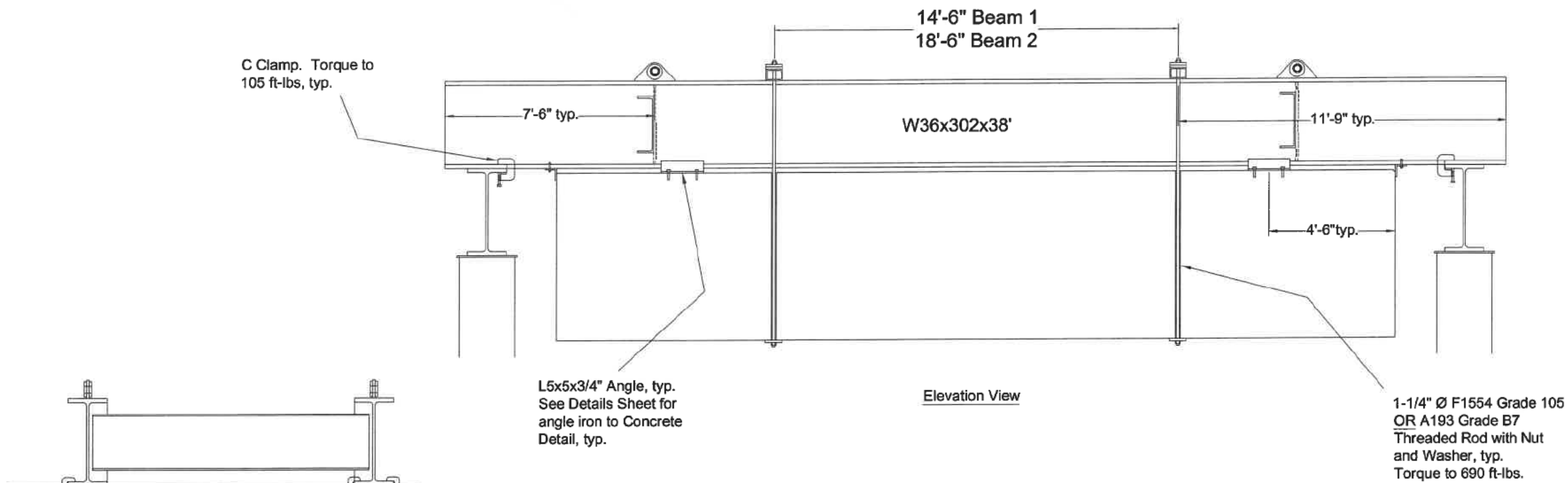
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Pacific Pile

Skagway Ore Terminal Demolition  
Post Tensioned Beam Support



The anchors for the Anchor Beam are Unique. See Anchor Beam Sheet for details of how to connect the Anchor beam to the concrete and how to connect the anchor beam to the cap beams. The anchor beam is the beam closest to the sawcut line as shown.



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Pacific Pile

Skagway Ore Terminal Demolition

Section 1 & 5 Falsework Details

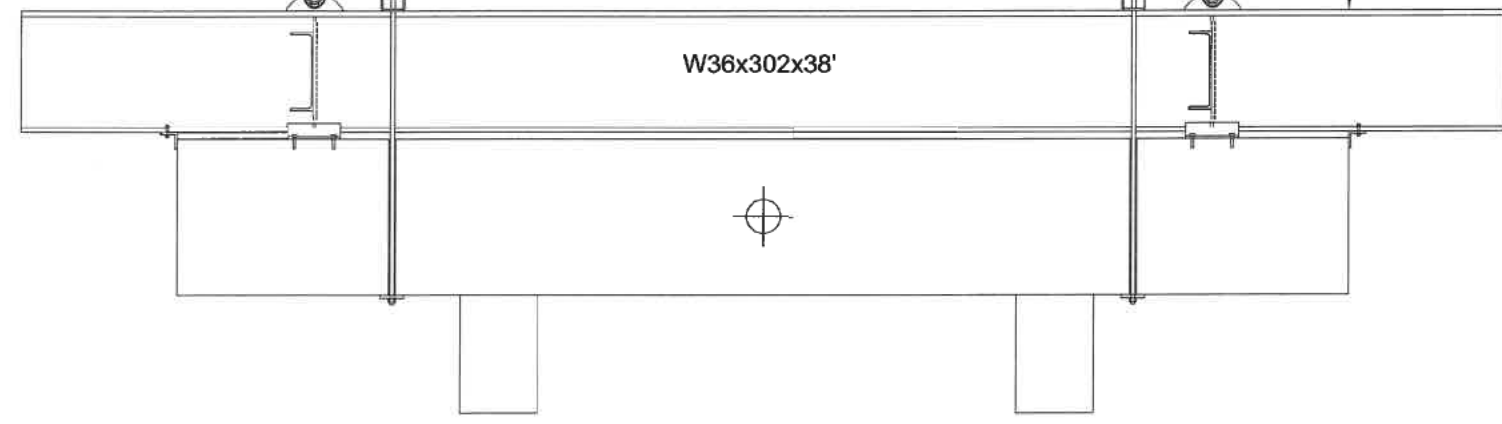
Sheet 8 of 16

4 point rigging.  
 Max angle from horizontal = 60°  
 Rated capacity at 60° max angle = 431 kips  
 Maximum leg load is 125 kips

21'-9 1/2" ±

2-1/2" Alloy Bolt Type shackle, typ.

Padeye, typ.  
 See details sheet

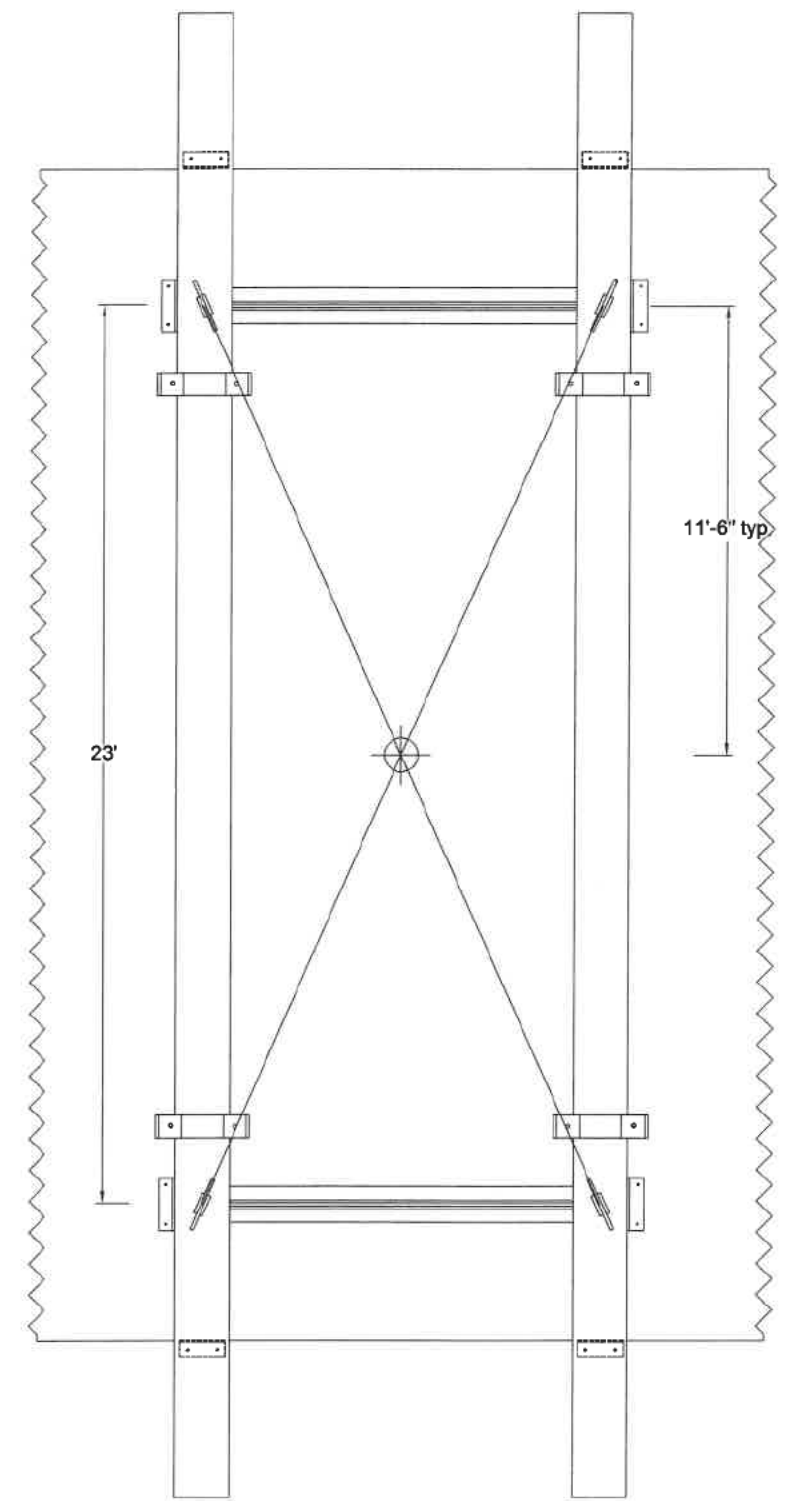
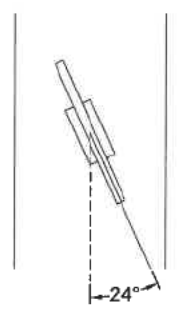


W36x302x38'

Elevation View

Notes:  
 Estimated Pick weight: 431 kips  
 Includes: Concrete at 160 pcf  
 36" beams with bracing, rods, and tubes  
 5' of existing piles attached to concrete

Angle padeye  
 towards pick point



12/31/23

NO.	DATE	REVISIONS	DESIGNED & DRAWN BY
0	08/28/2023		
1	9/13/2023		
			CHECKED BY
			DATE

**BIMIA**  
 BMA Construction Engineers Inc.  
 1515 Oakland Blvd. Suite 220  
 Walnut Creek, Ca 94596  
 www.thebmacorp.com

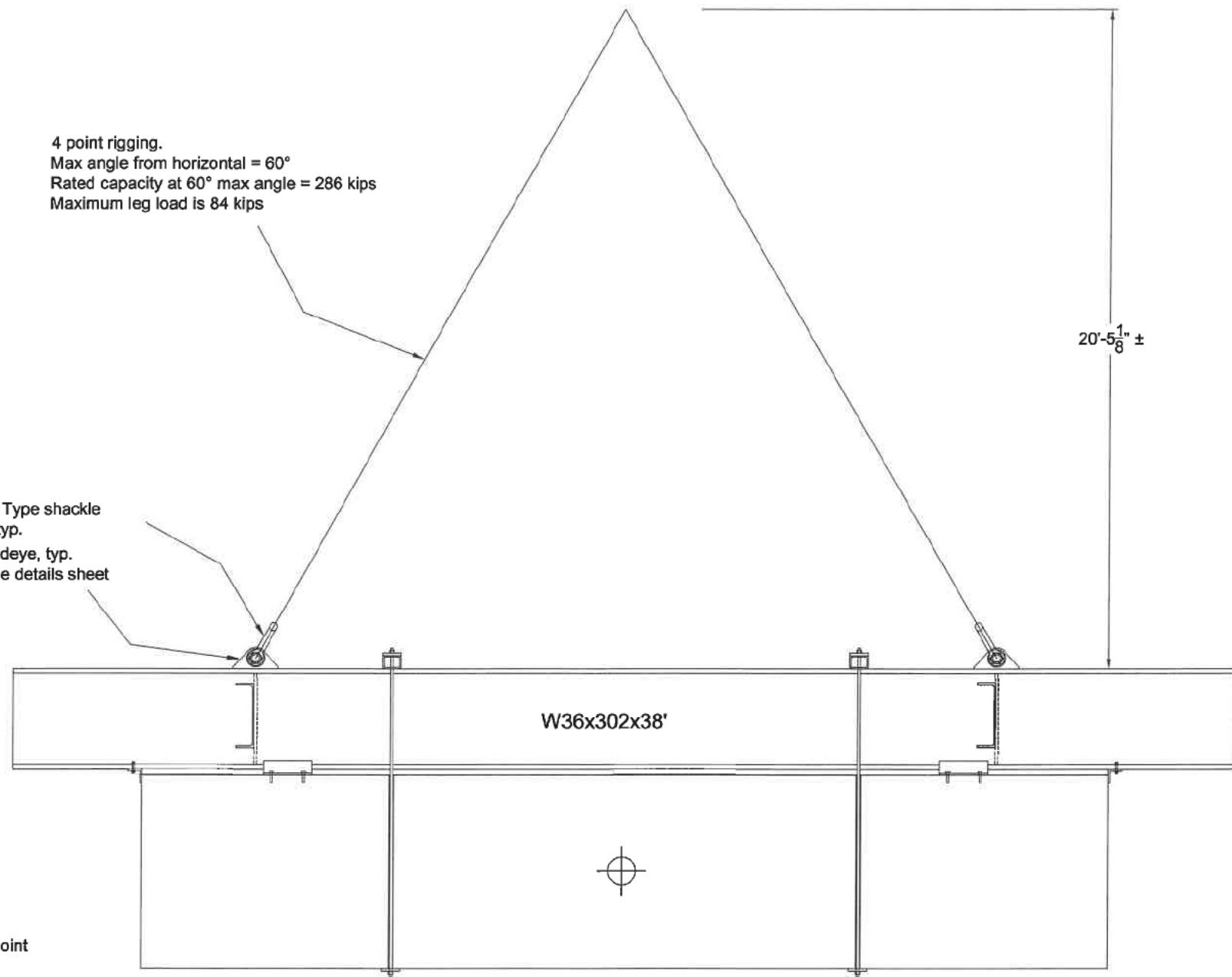
Pacific Pile

Skagway Ore Terminal Demolition  
 Section 2-4 Rigging Details

4 point rigging.  
 Max angle from horizontal = 60°  
 Rated capacity at 60° max angle = 286 kips  
 Maximum leg load is 84 kips

2-1/2" Alloy Bolt Type shackle  
 85 Tn capacity, typ.  
 Padeye, typ.  
 See details sheet

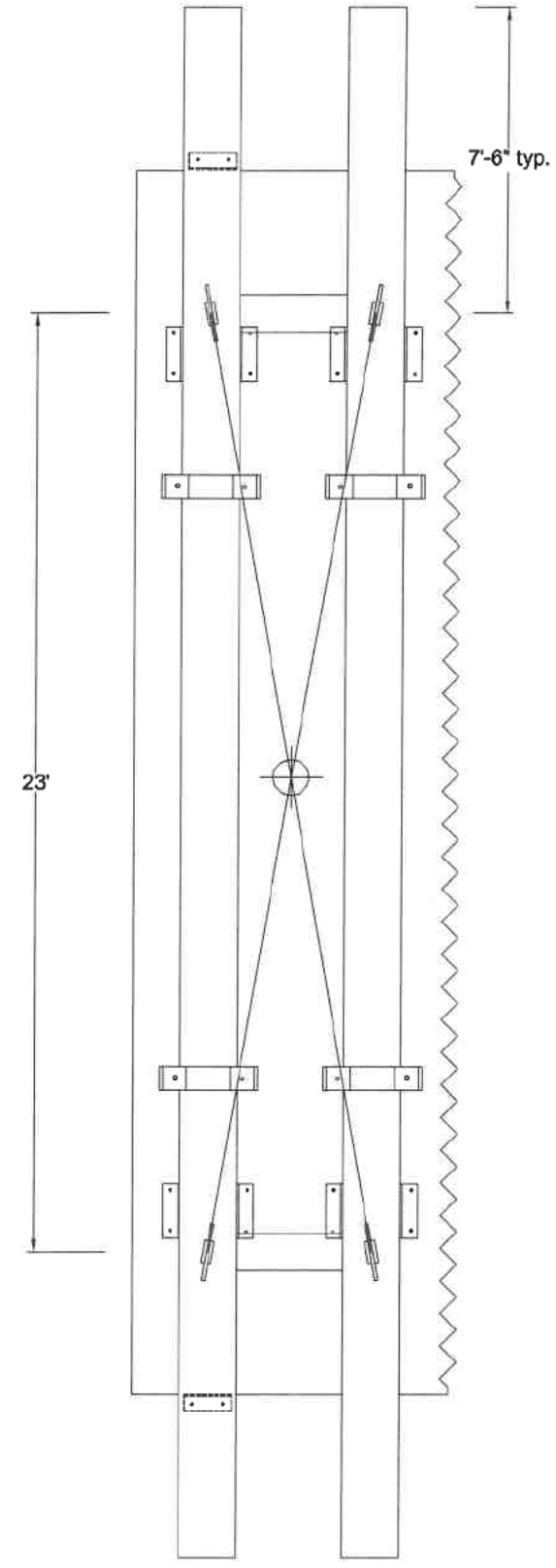
Angle padeye  
 towards pick point



W36x302x38'

Elevation View

Notes:  
 Estimated Pick weight: 286 kips  
 Includes: Concrete at 160 pcf  
 36" beams with bracing, rods, and tubes  
 5' of existing piles attached to concrete



Plan View



12/31/23

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			DATE



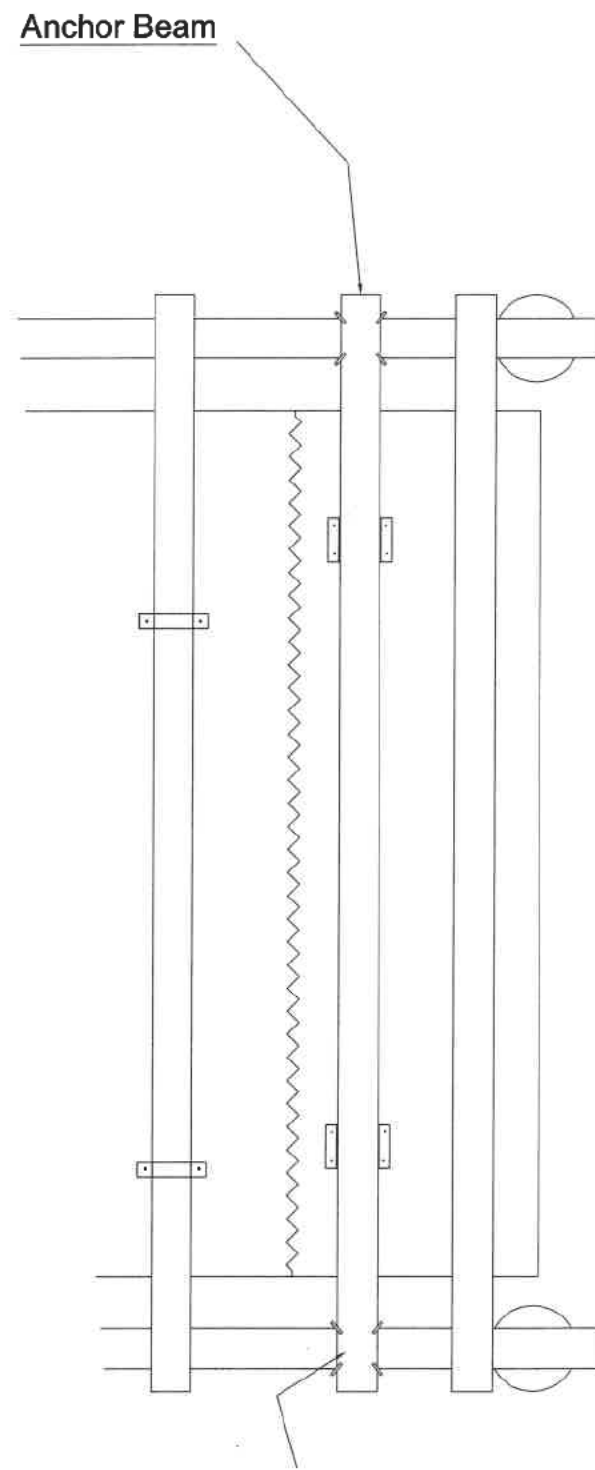
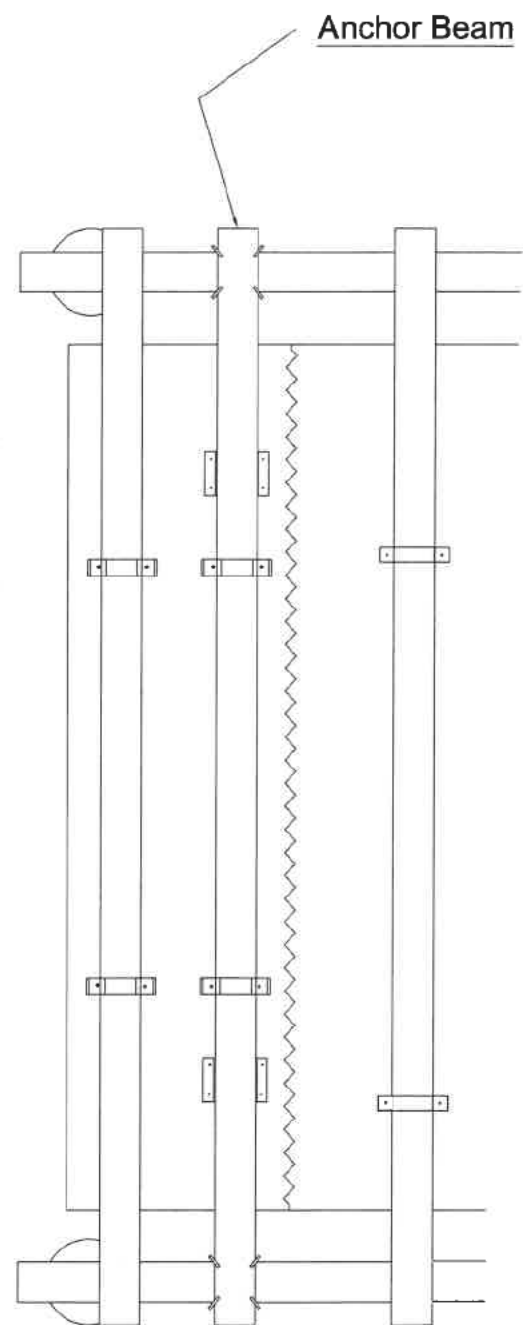
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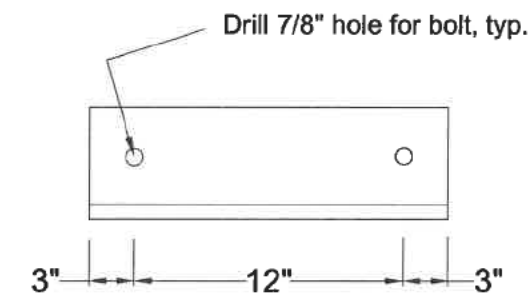
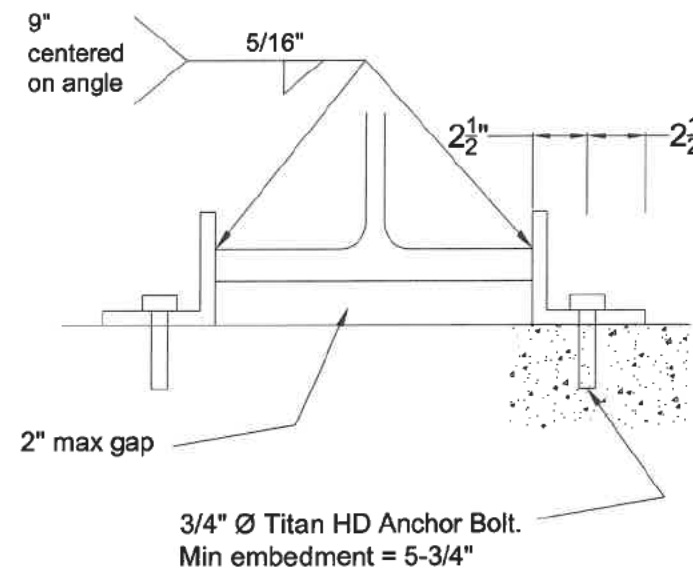
Skagway Ore Terminal Demolition

Section 1 & 5 Rigging Details

Sheet 10 of 16



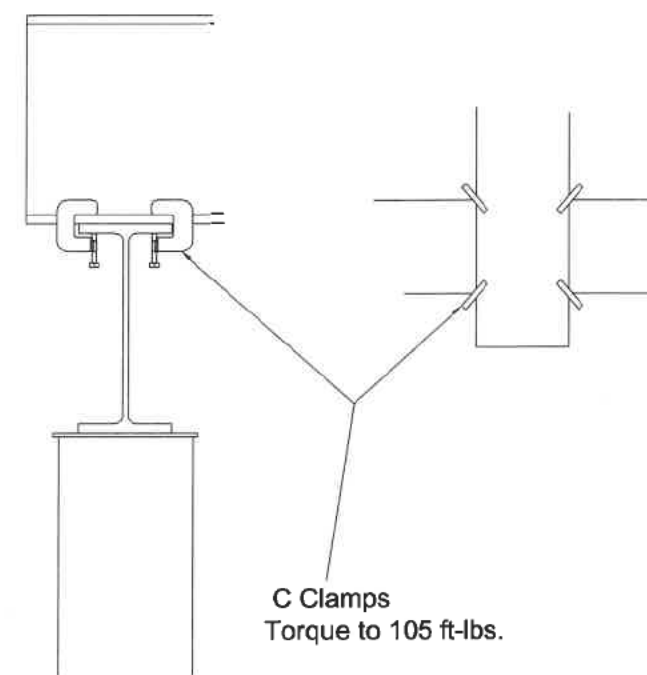
4 ea. C Clamps each end.  
Torque to 105 ft-lbs.



**L5x5x3/4" angle**

**IMPORTANT NOTE:**

THE 5/16" WELD SHOWN BETWEEN THE BEAM AND THE ANGLE CANNOT BE INSTALLED UNTIL THE RODS SUPPORTING THE CONCRETE SLAB ARE PRELOADED PER THESE PLANS.



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Skagway Ore Terminal Demolition

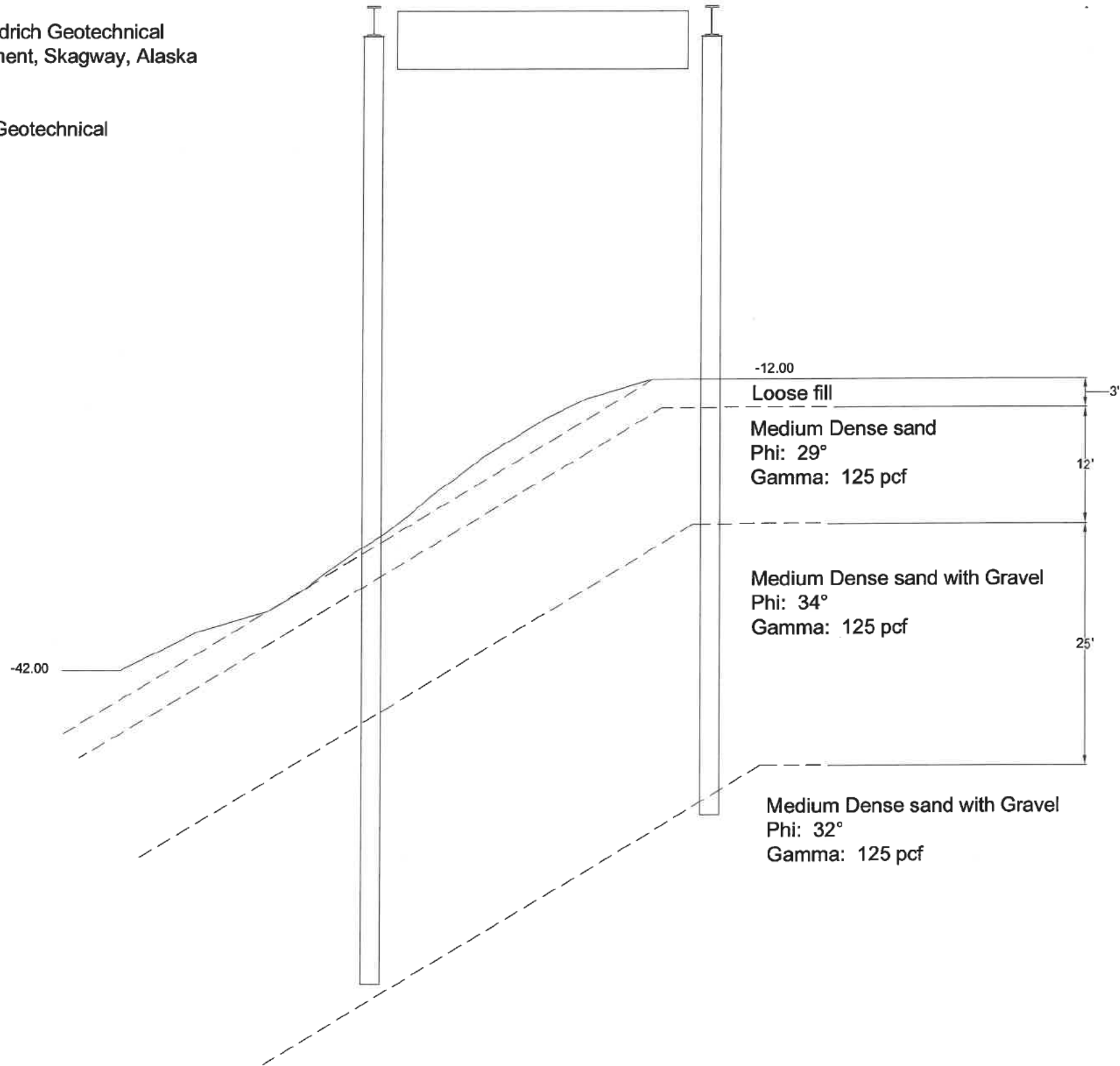
Anchor Beams

Sheet 11 of 16

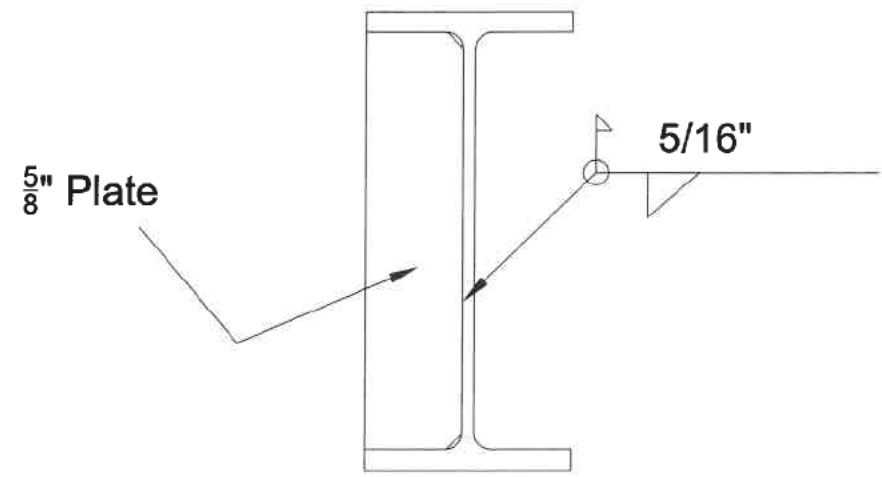
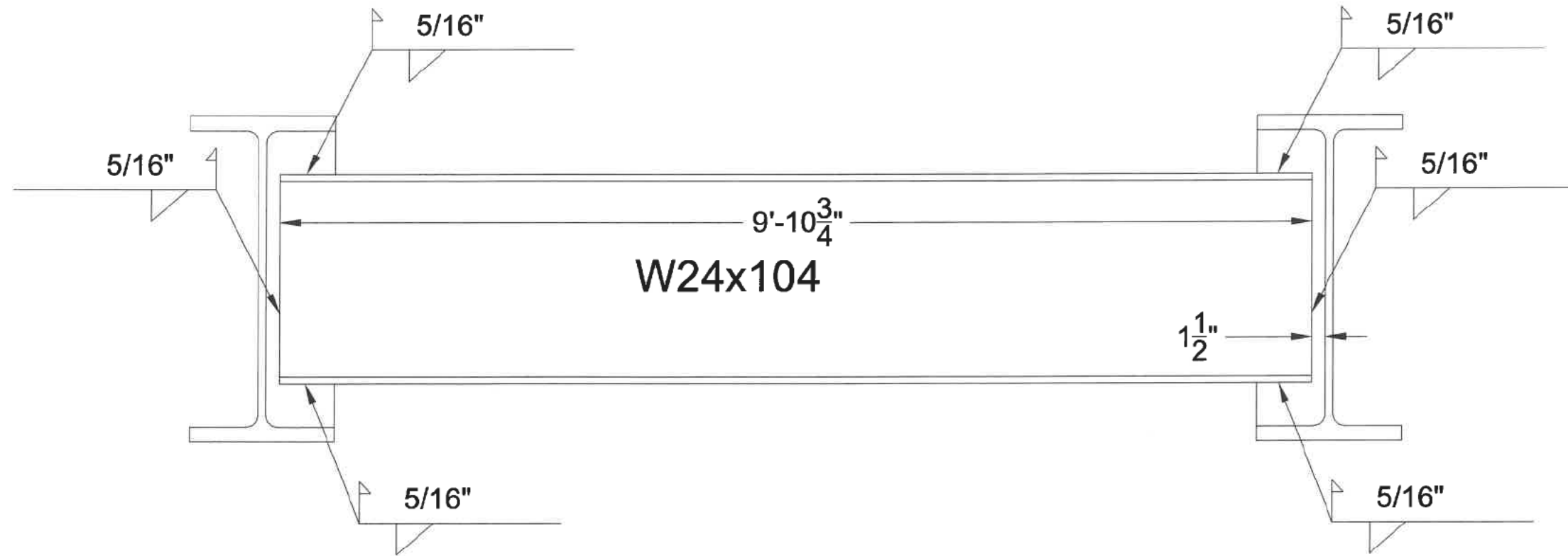


Based on Boring G-01 from Haley Aldrich Geotechnical  
 Report on Ore Peninsula redevelopment, Skagway, Alaska  
 Dated June 1, 2023

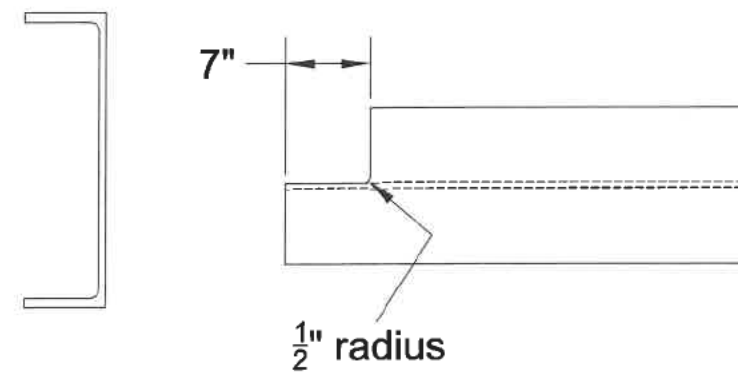
All soils considered submerged per Geotechnical  
 recommendations.



NO.	DATE	REVISIONS	DESIGNED & DRAWN BY	 BMA Construction Engineers Inc. 1515 Oakland Blvd. Suite 220 Walnut Creek, Ca 94596 www.thebmacorp.com	PACIFIC PILE	Skagway Ore Terminal Demolition	Soil Profile	Sheet 12 of 16
0	8/28/2023		CHECKED BY					
			DATE					



**Stiffener**



**W24x104 Coping Details**



NO.	DATE	REVISIONS	DESIGNED & DRAWN BY
0	08/28/2023		
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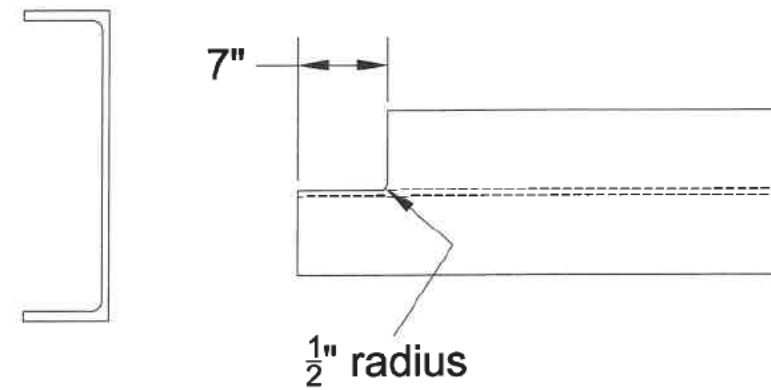
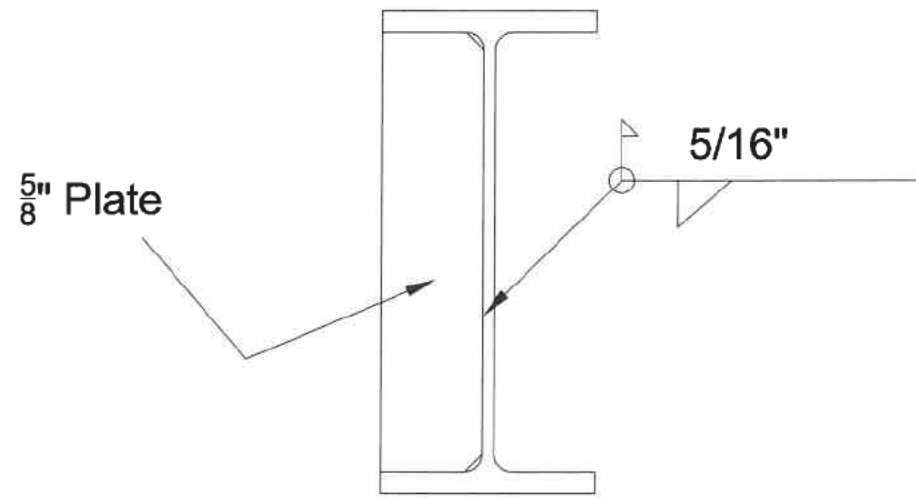
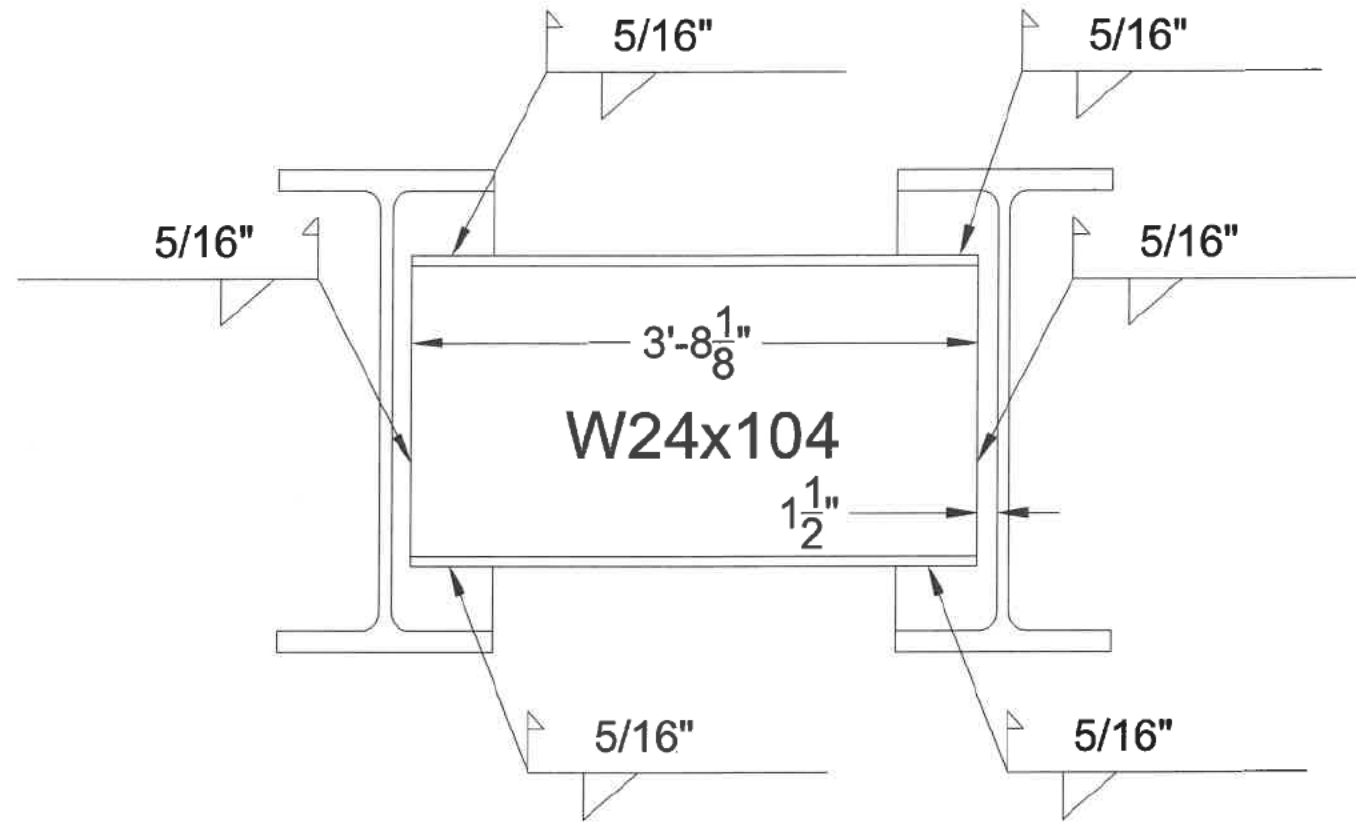


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Walnut Creek, Ca 94596  
www.themacorp.com

**Pacific Pile**

Skagway Ore Terminal Demolition

Bracing Details Sheet  
Sections 2-4



Stiffener

W24x104 Coping Details



NO.	DATE	REVISIONS	DESIGNED & DRAWN BY
0	08/28/2023		
1	9/13/2023		
			DATE

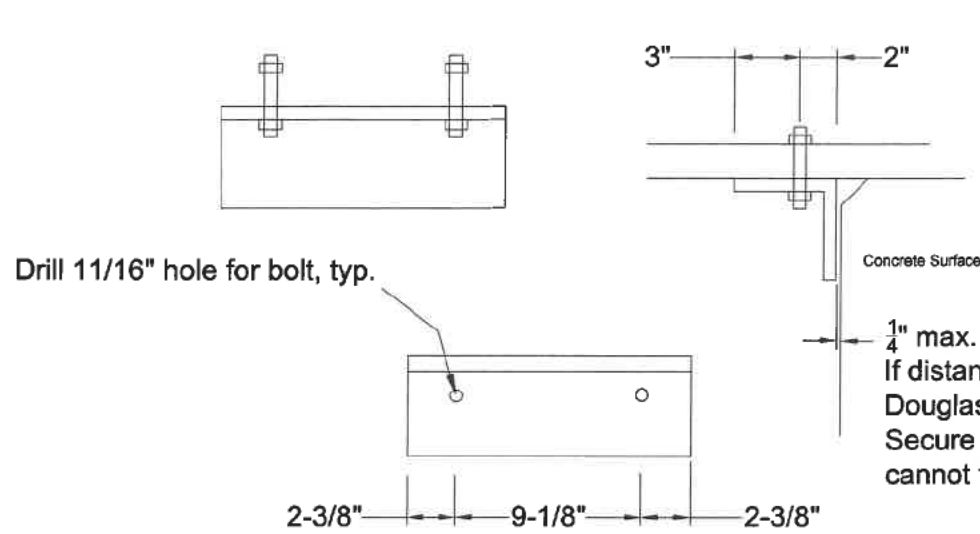


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Pacific Pile

Skagway Ore Terminal Demolition

Bracing Details Sheet  
Sections 1 and 5

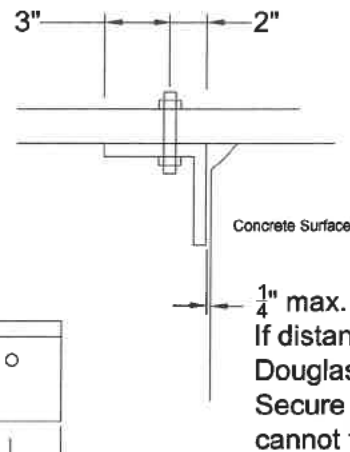


$\frac{5}{8}$ "  $\varnothing$  A325 bolt snug tight with nuts, typ.  
Drill  $\frac{11}{16}$ " hole in flange for bolt.

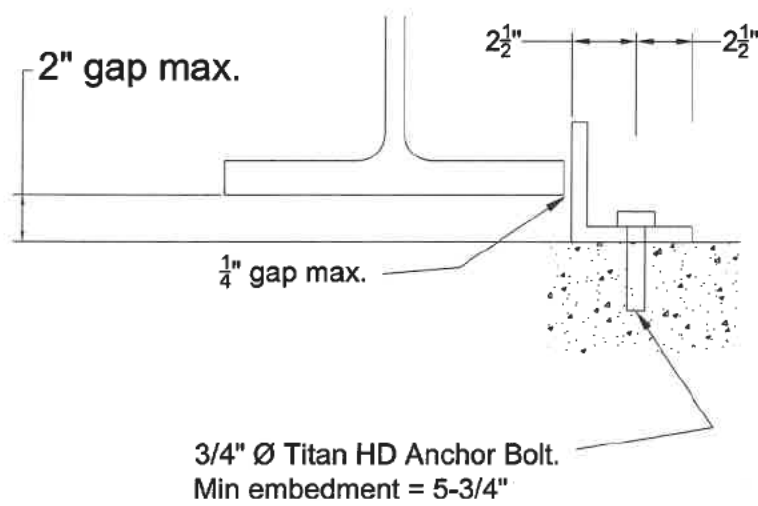
Drill  $\frac{11}{16}$ " hole for bolt, typ.

2-3/8" 9-1/8" 2-3/8"

L5x5x3/4" Angle Connected to Beam Flange

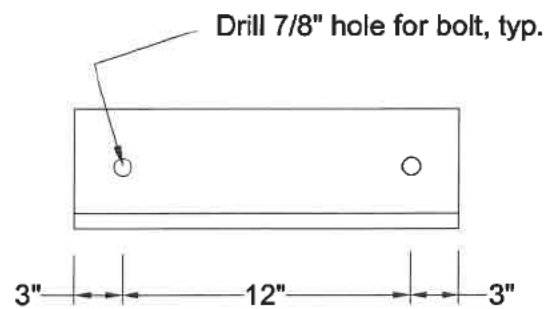


Concrete Surface  
 $\frac{1}{4}$ " max.  
If distance is larger, shim with Douglas Fir or Steel Shims. Secure shims so that they cannot fall out.



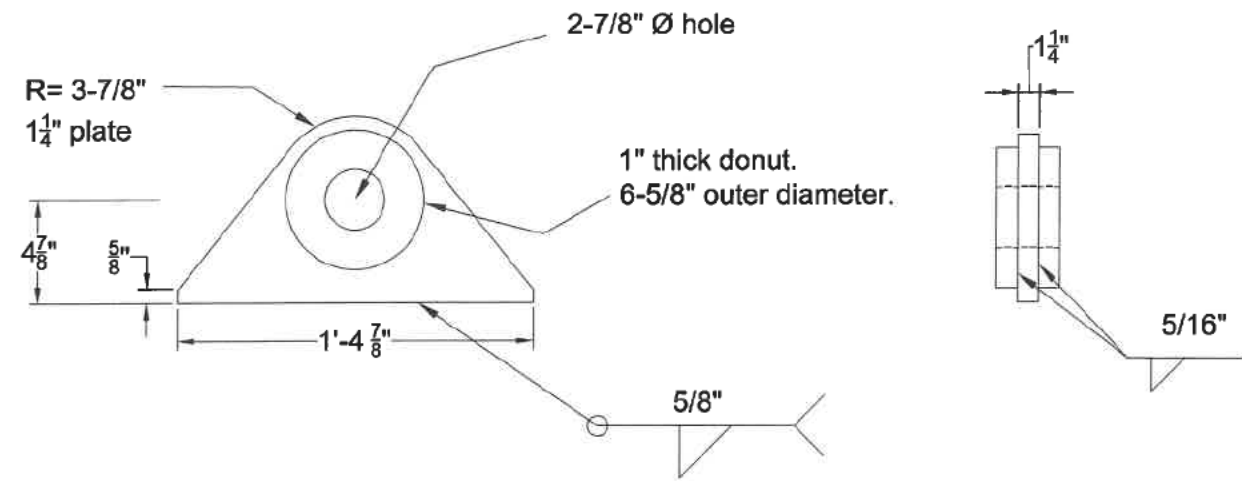
2" gap max.  
 $\frac{1}{4}$ " gap max.  
3/4"  $\varnothing$  Titan HD Anchor Bolt.  
Min embedment = 5-3/4"

L5x5x3/4" Angle Detail to Concrete

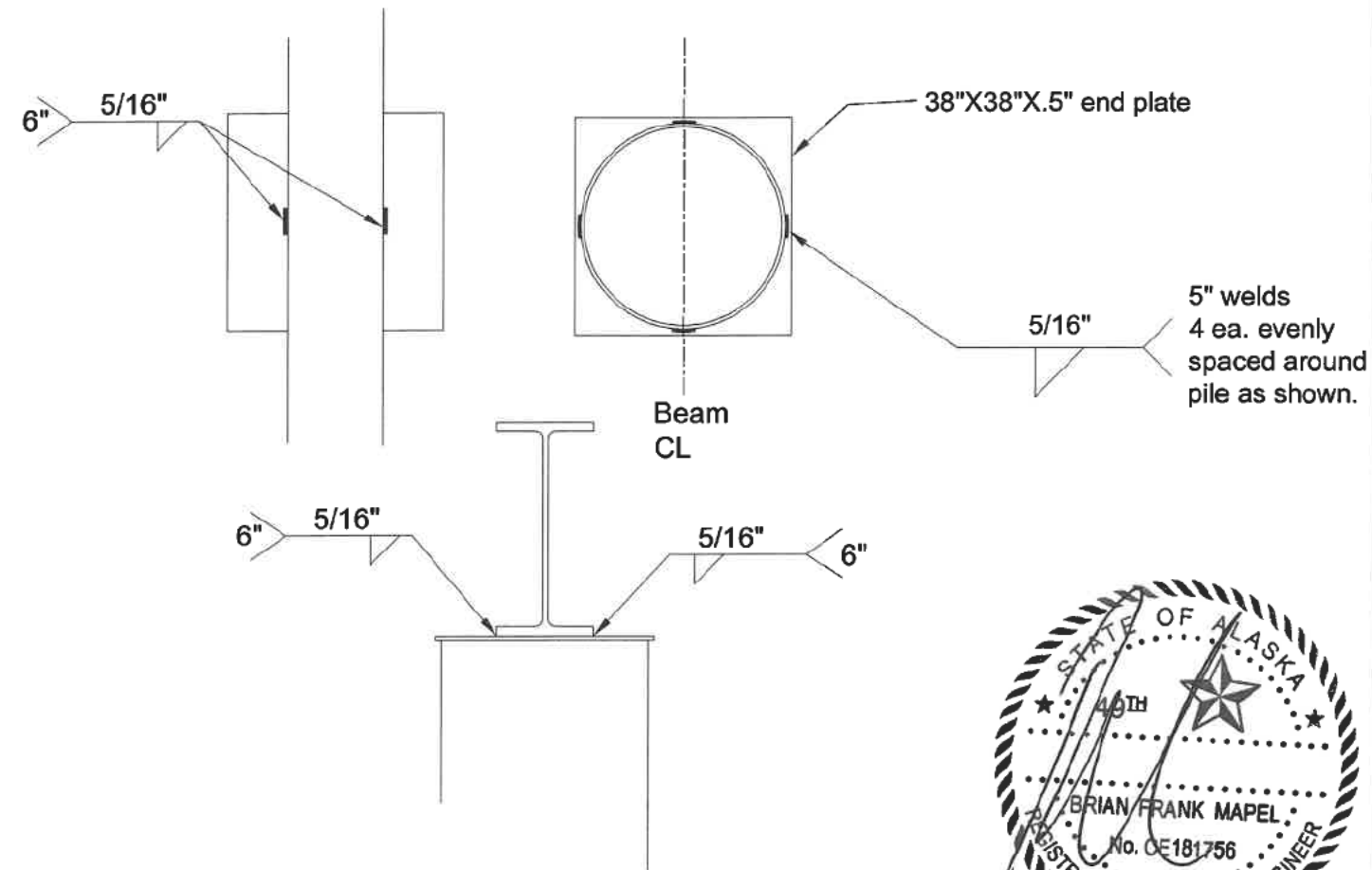


Drill 7/8" hole for bolt, typ.

3" 12" 3"



Padeye Detail



Pile Cap/Cap Beam Connections



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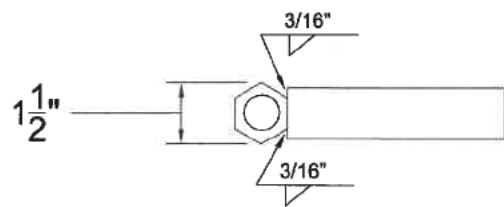
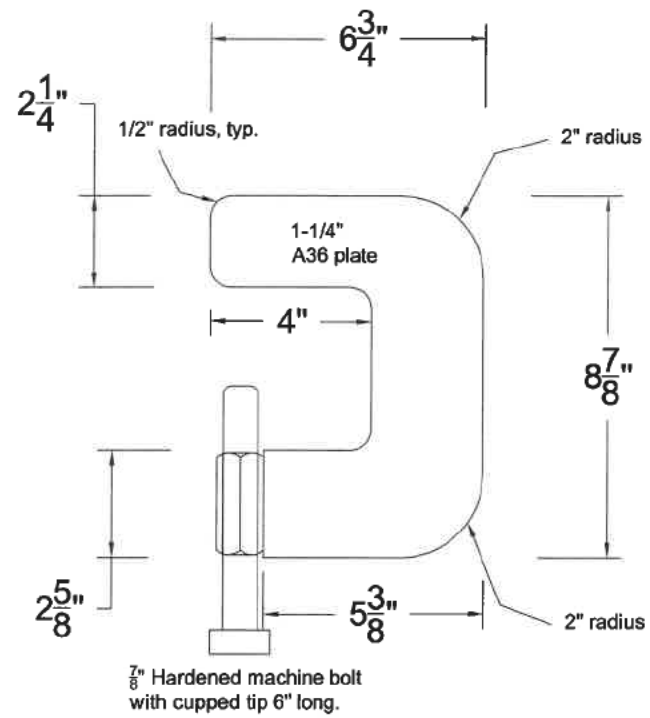
Pacific Pile

Skagway Ore Terminal Demolition

Bracing Details Sheet

Sheet 15 of 16

# Large C-Clamp



NO.	DATE	REVISIONS	DESIGNED & DRAWN BY
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			DATE



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Pacific Pile

Skagway Ore Terminal Demolition

Bracing Details Sheet

Sheet 16 of 16

**Attachment 2:  
PCO 032 - Ore Loader Platform Concrete Delamination**

**Ore Peninsula Redevelopment Project**

**OWNER:** Municipality of Skagway  
**DESC:** PCO 032 - Ore Platform Delamination

**START DATE:** November 18, 2023  
**FINISH DATE:** December 15, 2023

**COST SUMMARY**

DESCRIPTION OF WORK	COST TYPE	QUANT.	UNITS	UNIT RATE	LABOR (LAB)	SUPPLIES (SUP)	PPM EQUIP (PPM)	RENT EQUIP (RENT)	SUBCONT (SUB)	PERM MAT'L (PM)
Fabricate Support Beams, 11/19/23	L,E	1.0	LS	\$ 397.19	\$ 387.87		\$ 9.33			
Fabricate Support Beams, 11/20/23	L,E	1.0	LS	\$ 1,775.31	\$ 1,660.71		\$ 114.59			
Install Support Beams, 11/21/23	L,E	1.0	LS	\$ 1,836.05	\$ 1,542.99			\$ 293.06		
Install Support Beams, 11/22/23	L,E	1.0	LS	\$ 7,837.11	\$ 2,184.84		\$ 85.60	\$ 5,566.67		
Install Support Beams, 11/28/23	L,E	1.0	LS	\$ 4,656.63	\$ 2,670.47		\$ 1,595.41	\$ 390.75		
Central Environmental Inc. - Additional Core Drilling for Box Beam Supports	S	1.0	LS	\$ 22,998.00					\$22,998	
Brian Mapel Engineering - Engineering of Box Beam Supports	S	1.0	LS	\$ 1,800.00					\$1,800	
Materials	M	1.0	LS	\$ 5,940.81		\$5,941				

This change order request includes costs incurred as a result of the Differing Site Condition experienced while demolishing the ore loader platform concrete. The beams below the soffit of the ore loader platform began to delaminate and required additional work to stabilize the concrete beams during demolition by the use of engineered box beam supports. Please reference RFI 102 and SL 016 for further details.

PPM placed the concrete platform sections in the Broadway yard to make available for inspection upon further demolition. PPM and KPFF representatives observed that the #7 bars were not present to the extent shown in the as-built drawings, and therefore conclude this impact as caused by a differing site condition.

PPM reserves the right to request an extension of time for schedule impacts related to this differing site condition.

	LABOR	SUPPLIES	PPM EQUIP	RENT EQUIP	SUBS	PERM MATERIAL
<b>SUBTOTALS:</b>	\$8,446.87	\$5,940.81	\$1,804.93	\$6,250.48	\$24,798.00	\$0.00
Taxes @: 0.00%						
OH and Fee	15.0%	10.0%	10.0%	10.0%	10.0%	10.0%
Markup Totals	\$1,267.03	\$594.08	\$180.49	\$625.05	\$2,479.80	\$0.00
<b>ITEM TOTALS</b>	<b>\$9,713.91</b>	<b>\$6,534.89</b>	<b>\$1,985.43</b>	<b>\$6,875.52</b>	<b>\$27,277.80</b>	<b>\$0.00</b>

<b>TOTAL AMOUNT</b>	<b>\$52,387.55</b>
<b>Bond</b>	<b>1.0%</b>
<b>SUBTOTAL</b>	<b>\$52,911.42</b>
<b>GRAND TOTAL</b>	<b>\$52,911.42</b>



Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009

Date Work Performed: 11/20/2023

Description: Ore loader platform concrete delamination of beam from cap. PPM is fabricating support beams to secure the concrete beam to cap, to allow for safe removal and protect equipment from damage.

Phase Code: \_\_\_\_\_

	Craft Code	QUANTITY			UOM	NOTES
		RT	OT	DT		
LABOR		HR	HR	HR		
		6				Cutting spacers
		4				Beam prep
		4				Beam prep
		1				Supervise
		0.5				Admin Tracking
EQUIPMENT						
	EQ #					
Hyster 50 Forklift		0.5				
Torch (2)		10				
10K Forklift 1044		1				
INVOICE	QTY	UNIT				
MATERIALS						
Box Beam 48"x6"x6"		24				
INVOICE	QTY	UNIT				
SUBCONTRACTORS						
Concrete Coring Co.						
INVOICE	QTY	UNIT				
SERVICES						

OWNER REPRESENTATIVE *William Hammer* 11-20-2023  
PPM REPRESENTATIVE *[Signature]* 11-20-23  
Date



Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009

Date Work Performed: 11/21/2023

Description: Ore loader platform concrete delamination of beam from cap. PPM is fabricating support beams to secure the concrete beam to cap, to allow for safe removal and protect equipment from damage.

Phase Code: \_\_\_\_\_

		QUANTITY			UOM	
		RT	OT	DT		
LABOR		HR	HR	HR		NOTES
[Redacted]	[Redacted]	4				Prep and install
[Redacted]	[Redacted]	3				Install
[Redacted]	[Redacted]	3				Install
[Redacted]	[Redacted]	3				Install/Supervise
[Redacted]	[Redacted]	1				Admin Tracking/Support
	EQ #					
EQUIPMENT						
135 Genie Man lift		3				
		INVOICE	QTY	UNIT		
MATERIALS						
12' All-thread rod 1.25" SS			8			
Nuts SS			16			
1/2" Rope			160'			
		INVOICE	QTY	UNIT		
SUBCONTRACTORS						
Concrete Coring Co						
		INVOICE	QTY	UNIT		
SERVICES						

OWNER REPRESENTATIVE

*William Hammac*

11-21-2023

Date

PPM REPRESENTATIVE

*JL Peterson*

11-21-23

Date



**Skagway Ore Peninsula Redevelopment  
Pacific Pile and Marine Project No. 23009**

**Date Work Performed:** 11/28/2023

**Description:** Ore loader platform concrete delamination of beam from cap. PPM is fabricating support beams to secure the concrete beam to cap, to allow for safe removal and protect equipment from damage.

**Phase Code:** \_\_\_\_\_

Craft Code	QUANTITY			UOM	NOTES
	RT	OT	DT		
<b>LABOR</b>	<b>HR</b>	<b>HR</b>	<b>HR</b>		
	4				Install
	4				Install
	4				Install/Supervise
	0.5				Admin Tracking
	4				Crane Operator
	4				Install
	4				Install
	EQ #				
<b>EQUIPMENT</b>					
Genie 135 XC Manlift	4				
9299 Crane	4				
Chainsaw	0.5				Cut away wood piles
Torch	0.5				Cut away steel piles
	INVOICE	QTY	UNIT		
<b>MATERIALS</b>					
12' All-thread rod 1.25" SS	8				
Nuts SS	16				
1/2" Rope	160'				
	INVOICE	QTY	UNIT		
<b>SUBCONTRACTORS</b>					
Concrete Coring Co					
	INVOICE	QTY	UNIT		
<b>SERVICES</b>					

OWNER REPRESENTATIVE

*William Hammock*

11-28-2023

Date

PPM REPRESENTATIVE

*[Signature]*

11-28-23

Date

INVOICE NO. 13864



Environmental Services

229 E. Whitney Road, Anchorage, AK 99501  
(907) 561-0125 ☎ FAX (907) 561-0178

**SOLD TO:**

Pacific Pile and Marine LP  
700 South Riverside Drive  
Seattle WA 98108

**PROJECT DESCRIPTION/LOCATION:**

Ore Peninsula Redevelopment

INVOICE DATE	INVOICE NO.	PROJECT NO.	CONTRACT NO.	PROGRESS BILLING
1/16/2024	13864	14480	23009	03

**CONTRACT INVOICE**

<b>ORIGINAL CONTRACT AMOUNT:</b>	<b>\$1,235,840.00</b>
<b>CHANGES OR AMENDMENTS:</b>	<b>\$64,813.00</b>
<b>ADJUSTED CONTRACT AMOUNT</b>	<b>\$1,300,653.00</b>
<b>CONTRACT COMPLETE TO DATE (72%)</b>	<b><u>\$1,247,131.00</u></b>
<b>LESS Retained (0%)</b>	<b>(\$-0-)</b>
<b>LESS PREVIOUS PAYMENTS</b>	<b><u>(\$934,132.00)</u></b>
<b>TOTAL DUE THIS INVOICE</b>	<b>\$312,999.00</b>

**TERMS: NET CASH DUE AND PAYABLE 10TH OF FOLLOWING MONTH  
NET 30 DAYS, FINANCIAL CHARGES OF 1-1/2% PER MONTH  
WILL BE CHARGED ON ALL PAST DUE ACCOUNTS.**

**Thank You**

Ore Peninsula Redevelopment Project - Demolition  
Project #23009

Central Environmental, Inc.

Bid ITEM	PPM Bid Item	Description	Total Cost	Percent Complete	Total Due	
		Ore Loader Demo: Abate and Recover				
60	61.01	Work	\$ 458,827.00	100%	\$ 458,827.00	
60	63.015	Create Work Plan for Seattle Disposal	\$ 5,000.00	0.00	\$ -	Not Performed
60	63.016	Cut up Ore Loader and Dispose in Seattle	\$ 3,823.00	0.00	\$ -	Not Performed
		Cut up Ore Loader and Disps - Non-				
60	63.04	Ferrous Materials	\$ 147,736.00	100%	\$ 147,736.00	
60	64.01	Demo Vacuum System	\$ 97,116.00	100%	\$ 97,116.00	
60	64.02	Demo Ore Loader Sumps	\$ 19,423.00	100%	\$ 19,423.00	
60	64.04	Containerize Residual Product	\$ 40,246.00	100%	\$ 40,246.00	
70	709.04	Marine Demo Existing Concrete Dock	\$ 197,950.00	100%	\$ 197,950.00	
		Marine Demo Concrete Ore Loader				
70	711.02	Platform	\$ 216,020.00	100%	\$ 216,020.00	
		Asbestos Air Monitoring (Daily)	\$ 450.00	1.00	\$ 450.00	
		Lead Air Monitoring (Daily)	\$ 650.00	7.00	\$ 4,550.00	
<b>Change Orders</b>						
CO#1		Additional core holes and saw cuts	\$ 41,815.00	100.00%	\$ 41,815.00	
CO#2		Additional core holes in platform	\$ 22,998.00	100.00%	\$ 22,998.00	PCO 032
<b>Total Billing To date</b>					\$ 1,247,131.00	
<b>Total Previous Billing</b>					\$ (934,132.00)	
<b>Total Due</b>					\$ 312,999.00	



700 South Riverside Drive  
 Seattle, WA 98109  
 Office: 206.331.3873  
 Fax: 206.774.5958

Approved By \_\_\_\_\_

**SUBCONTRACTOR/SUPPLIER CHANGE ORDER**

Subcontractor/Supplier Name:	Central Environmental, Inc.	Change Order No.:	02
Address:	229 E. Whitney Rd. Ste 200	Change Order Date:	11/20/23
	Anchorage, AK 99501	Contract/P.O. Date:	08/30/23
Phone:	907-561-0125		
Project Name: (If Applicable)	Ore Peninsula Redevelopment		

This Change Order shall modify the Agreement between Pacific Pile & Marine, LP ("Contractor") and Central Environmental, Inc. entered into on 8/30/2023. (the "Contract").

**Description of Changes:**

24 additional 2" core holes in Platform concrete to support delaminating beam at soffit

**Attachments:**

Except for such changes as are set forth herein, all of the terms and conditions of the aforementioned Contract, and as it may have been heretofore modified in writing, shall be and remain the same.

The Original Contract Sum:	\$ 1,235,840.00
Net change by previously authorized Change Orders:	\$ 41,815.00
The Contract Sum prior to this Change Order was:	\$ 1,277,655.00
This Change Order will <u>increase</u> decrease (circle one) the the Contract Sum by:	\$ 22,998.00
The new Contract Sum including this Change Order will be:	\$ 1,300,653.00
Time Extension to Contract Time (if applicable):	N/A

The Change Order is accepted upon the terms and conditions stated above and those in the original Contract. This Change Order is not valid until signed by both Pacific Pile & Marine, LP and Subcontractor/Supplier, and a signature indicates the party's agreement herewith, including any adjustment in Contract Sum or Contract Time. Subcontractor/Supplier accepts this Change Order as full payment for any and all claims relating to the work covered by this Change Order, including but not limited to claims for time extensions, direct and indirect costs, delay, impact, inefficiency, and accelerations costs. For Unit Price Contracts, all extended dollar amounts (i.e., unit price multiplied by anticipated quantities) are approximate based upon anticipated quantities, and the actual extended amount will be determined by extension of unit prices and actual quantities as measured according to the Contract provisions.

**PACIFIC PILE & MARINE, LP.**

X \_\_\_\_\_  
 Digitally signed by James Davidson  
 DN: C=US,  
 E=jimd@pacificpile.com,  
 CN=James Davidson  
 Date: 2023.11.21 14:15:01-08'00'  
 By: James Davidson  
 (Please print or type)  
 Title: \_\_\_\_\_  
 Date: \_\_\_\_\_

**CENTRAL ENVIRONMENTAL, INC.**

X \_\_\_\_\_  
 By: Tali Landau  
 (Please print or type)  
 Title: Project Manager  
 (Please print or type)  
 Date: 11/21/23



1515 Oakland Blvd.  
 Suite 220  
 Walnut Creek, Ca.  
 94596  
 Phone: 925-387-0380  
 Fax: 925-387-0732

# Invoice

Date	Invoice #
11/18/2023	PP-008

Bill To
Pacific Pile 700 South Riverside Dr. Seattle, Wa. 98108

P.O. No.	Terms	Project
	Net 30	

Quantity	Description	Rate	Amount
	Final Invoice for Skagway Demolition Engineering Middle Tower removal Plan Misc. mods during removal operations		
18	Senior Engineer	210.00	3,780.00
64	Junior Engineer	120.00	7,680.00
	SEE CHRIS LUNDFELDT FOR APPROVAL SKAGWAY PROJECT		
	Misc mods during ore platform removal, box beam support engineering: 4 hrs Senior Engr x \$210/hr = \$840 8 hrs Junior Engr x \$120/hr = \$960 \$1800 Total PCO 32		
		<b>Total</b>	\$11,460.00



**Seaport Steel & Oregon Metal Services**

Varsteel Family of Companies  
 3660 E. Marginal Way S.  
 Seattle, WA 98134  
 Phone: 206-343-0700  
 Fax: 206-343-1366  
 Website: www.seaportsteel.com or  
 www.oregonmetal.com

**Invoice No: 341907**

**Bill To:** PACIFIC PILE AND MARINE LP  
 700 S. RIVERSIDE  
 SEATTLE, WA 98108

**Ship To:** PACIFIC PILE & MARINE LP  
 700 S. RIVERSIDE  
 SEATTLE, WA 98108

Customer P.O.#: 30750

Sales Order No: 315098

**Invoice Date:** 9/25/23

Contract:

F.O.B.: Destination

Ship Via: Our Truck

Sales 1: Melodie Ulsh

Sales 2: Sam Paulsen

Ship Date: 9/25/23

Terms: Net 30

Messages

RECEIVING HRS 7AM-330PM MON-THURS CALL ON  
 FRI

- \* PO# REQUIRED ON ALL INVOICES
- \* FORMAL PO DOCUMENT MUST BE ISSUED PRIOR TO ALLOWING CHARGES ON ACCOUNT
- \* JOB SITE CONTACT AND NUMBER REQUIRED FOR ALL JOB SITE DELIVERIES

**1" Plate**  
 \$0.84/lb x \$68.06 lb/sf x 12 total SF = **\$686.09**

BO Qty	Qty Shipped	UM	Description	Width	Length	Weight	Price	UM	Extension	Tax
----- BOL No: 330644 -----										
0	2	P	1 Plate-A36 A36	96"	240"	13,068.30	84.00	C	\$10,977.37	E
0	2	P	1/2 Plate-A36 A36	96"	240"	6,534.10	84.00	C	\$5,488.64	E
0	10	P	2X2X.250 Tube A500B		20' 0"	1,082.00	82.00	C	\$887.24	E
0	5	P	3/8X4 Flat Bar A572		20' 0"	510.00	73.00	C	\$372.30	E
0	5	P	1/2X6 Flat Bar A572		20' 0"	1,020.00	73.00	C	\$744.60	E
0	6	P	6X6X.375 Tube A500B		20' 0"	3,297.60	82.00	C	\$2,704.03	E
0	10	P	3X2X1/4 Angle A572		20' 0"	820.00	74.00	C	\$606.80	E
0	1		Fuel Surcharge					A	\$86.90	E
Total Weight						<b>26,332.00</b>				
<b>Subtotal Non taxable</b>									<b>\$21,867.88</b>	
<b>Subtotal taxable</b>									<b>\$0.00</b>	
<b>Total</b>									<b>\$21,867.88</b>	

**Box Tube**  
 \$0.82/lb X 27.48 lb/ft X 96 total LF = **\$2,163.28**





**INVOICE**

Invoice: 724124

<b>Sold To:</b> 68677605 PACIFIC PILE & MARINE 700 S. RIVERSIDE SEATTLE WA 98108 USA	<b>Ship To:</b> PACIFIC PILE & MARINE 700 SOUTH RIVERSIDE DRIVE Loren Bishop 206-947-3274 SEATTLE WA 98108 USA
--	---

PO Number: 30718	Terms: Net 30 Days	F.O.B.: PPD/SHIPPOINT
Sales Rep: Dylan Drennan	Order #: 315710	Ship Via: AMERICAN FREIGHTWAYS
Packing Slip: 21699		Ship Date: 9/29/2023

*Per DD*  
 PO: 30718  
 Project: 23009 / Skagway  
 PPD/AMERICAN#24565/PDX

Line	Part Number/Description	Quantity	Unit Price	Ext Price
1	R9F-10-436-ST-D-PF 1-1/4" F436 Hardened washer, Domestic, Plain Finish	96.00EA	1.43000	137.28
2	B8V-10-14400-RH-UN-D-PF 1-1/4"-8 UN B7 All-Thread Bar x 12'0", RH, Domestic, Plain Finish	23.00EA	116.35000	2,676.05
<i>Qty. Ordered: 48.00 EA</i>				
3	B8V-10-14400-RH-UN-D-PF 1-1/4"-8 UN B7 All-Thread Bar x 12'0", RH, Domestic, Plain Finish	25.00EA	116.35000	2,908.75
<i>Qty. Ordered: 48.00 EA</i>				
4	H1FU-10-HN-2H-RH-D-PF HEX NUT 2H, 1-1/4"-8 UN* RIGHT HAND, DOMESTIC, PLAIN FINISH	96.00EA	4.43000	425.28

<i>Miscellaneous Charges:</i>	
Description	Amount
Freight Charge - Taxable	1,176.47

**Nuts**  
 \$4.43/EA x 48 EA = \$212.64

**All-Thread**  
 \$116.35/12 LF x 24 EA =  
 \$2,792.40

**PLEASE REMIT TO**

WILLIAMS FORM ENGINEERING CORP  
 PO BOX 675350  
 DETROIT, MI 48267-5350

Line(s) Subtotal:	6,147.36
Miscellaneous Charges:	1,176.47
Less Advance Billing:	0.00
Total Tax:	750.69
Reverse Charge Tax:	0.00
Less Prepaid Deposits:	0.00
Less Deposit:	0.00
Rounding:	0.00
<b>Total</b>	<b>8,074.52</b>

# West Coast Wire Rope & Rigging Inc.

# INVOICE

Branch: 20

2900 NW 29th Avenue  
 PORTLAND, OR 97210  
 USA

PHONE: 503-228-9353  
 FAX: 503-228-2435  
 EMAIL: AR@WCWR.COM

INVOICE	
5610665	
Invoice Date	Page
09/27/2023 14:16:32	1 of 3
ORDER NUMBER	
1743564	

**PACIFIC PILE & MARINE LP [E]**  
 700 S RIVERSIDE DR  
 SEATTLE, WA 98108

**Ship To:**  
 PACIFIC PILE & MARINE LP [E]  
 WILL CALL WCWR-SEATTLE  
 SEATTLE, WA 98108

Customer ID: 17088

Term Description	Net Due Date	Disc Due Date	Discount Amount	Resale Certificate
NET 30	10/27/2023	10/27/2023	0.00	NONE

Order Date	Pick Ticket No	PO Number	Sales Rep
09/21/2023 15:38:32	3615572	30785	ASHLEIGHE

Quantities					Item ID	Pricing UOM	Unit Price	Extended Price
Ordered	Shipped	Remaining	UOM Unit Size	Disp.	Item Description	Unit Size		

**Delivery Instructions:** 4-5 DAYS ARO

**Carrier:** WILL CALL

**Tracking #:**

4.000	4.000	0.000	EA		WR1121LEGI	EA	320.0000	1,280.00
				1.0	1-1/2 SINGLE LEG ASSEMBLY	1.0		
					Wire length 30FT 6X26WS EIP IWRC RRL BRT A IMP WITH CARBON FLEMISHED 24 INCH EYES EACH END. TAG WITH WLL Serial/Cert#			
4.000	4.000	0.000	EA		WR1121LEGI	EA	235.0000	940.00
				1.0	1-1/2 SINGLE LEG ASSEMBLY	1.0		
					Wire length 13FT 6X26WS EIP IWRC RRL BRT A IMP WITH CARBON FLEMISHED 24 INCH EYES EACH END. TAG WITH WLL Serial/Cert#			
4.000	4.000	0.000	EA		SPA138D	EA	184.5000	738.00
				1.0	1-3/8 SCREW PIN ANCHOR SHACKLE GALV DOM 13-1/2 TON WLL	1.0		
4.000	4.000	0.000	EA		OL1.4	EA	11.5500	46.20
				1.0	4 P/P PURPLE POLY ROUND SLING	1.0		
4.000	4.000	0.000	EA		OL1.8	EA	19.6000	78.40
				1.0	8 P/P PURPLE POLY ROUND SLING	1.0		
4.000	4.000	0.000	EA		OL1.12	EA	21.4000	85.60
				1.0	12 P/P PURPLE POLY ROUND SLING	1.0		

ALL WIRE ROPE IS OF IMPORT ORIGIN UNLESS OTHERWISE SPECIFIED. ALL CLAIMS MUST BE UPON RECEIPT OF MERCHANDISE. NO UNAUTHORIZED RETURNS WILL BE ACCEPTED. NO REPRESENTATIONS OR WARRANTIES ARE MADE UNLESS SPECIFICALLY SET FORTH HERE IN WRITING. ALL RETURNS ARE SUBJECT TO OUR INSPECTION AND WE RESERVE THE RIGHT TO IMPOSE A 20% RESTOCKING CHARGE. WEST COAST WIRE ROPE ASSUMES NO RESPONSIBILITY FOR THE USE OR MISAPPLICATION OF ANY PRODUCTS SOLD BY THIS FIRM. OUR PRODUCTS ARE SOLD WITH THE EXPRESS UNDERSTANDING THAT THE PURCHASER OR USER IS THOROUGHLY FAMILIAR WITH THE CORRECT APPLICATION AND PROPER USE FOR WHICH IT IS BEING PURCHASED. WEST COAST WIRE ROPE IS NOT RESPONSIBLE FOR CUSTOMER SUPPLIED MATERIALS. WE RESERVE THE RIGHT TO IMPOSE A 1 1/2% (18% PER ANNUM) FINANCE CHARGE ON ALL PAST DUE INVOICES. FINANCE CHARGES WILL NOT BE INVOICED

\*\*\* REPRINT \*\*\*

# West Coast Wire Rope & Rigging Inc.

# INVOICE

Branch: 20

2900 NW 29th Avenue  
 PORTLAND, OR 97210  
 USA

PHONE: 503-228-9353  
 FAX: 503-228-2435  
 EMAIL: AR@WCWR.COM

INVOICE	
5610665	
Invoice Date	Page
09/27/2023 14:16:32	2 of 3
ORDER NUMBER	
1743564	

Quantities					Item ID	Pricing	Unit	Extended
Ordered	Shipped	Remaining	UOM	Unit Size	Item Description	UOM	Price	Price
4.000	4.000	0.000	EA	1.0	OL3.4 4 P/P YELLOW POLY ROUND SLING	EA	15.5500	62.20
4.000	4.000	0.000	EA	1.0	OL3.8 8 P/P YELLOW POLY ROUND SLING	EA	27.0000	108.00
4.000	4.000	0.000	EA	1.0	OL3.12 12 P/P YELLOW POLY ROUND SLING	EA	41.0300	164.12
2.000	2.000	0.000	EA	1.0	OL5.20 20 P/P RED POLY ROUND SLING	EA	88.5500	177.10
1,200.000	1,200.000	0.000	FT	1.0	RPBS034 3/4 3-STRAND BLUE STEEL POLY	FT	0.4200	504.00
<b>Lot Number:</b> CWC6081670-04					<b>Qty:</b>	600.000	FT	
<b>Lot Number:</b> CWC6081856-01					<b>Qty:</b>	600.000	FT	
3,000.000	3,000.000	0.000	FT	1.0	RPBS012 1/2 BLUE STEEL 3 STRAND ROPE	FT	0.1800	540.00
<b>Lot Number:</b> CWC6081647-02					<b>Qty:</b>	600.000	FT	
<b>Lot Number:</b> CWC6081647-03					<b>Qty:</b>	600.000	FT	
<b>Lot Number:</b> CWC6081647-04					<b>Qty:</b>	600.000	FT	
<b>Lot Number:</b> CWC6081647-05					<b>Qty:</b>	600.000	FT	
<b>Lot Number:</b> CWC6081647-06					<b>Qty:</b>	600.000	FT	
4.000	4.000	0.000	EA	1.0	CHGROM516 5/16 CHAIN SLING GROMMET	EA	135.5500	542.20
					Ordered length 6FT			
					Reach			
					Grade 100			
					Serial/Cert #			
4.000	4.000	0.000	EA	1.0	CHGROM038 3/8 CHAIN SLING GROMMET	EA	129.3000	517.20
					Ordered length 4FT			
					Reach			
					Grade 100			
					Serial/Cert #			
40.000	40.000	0.000	EA	1.0	RS3X40 3 IN X 40 FT RATCHET TIEDOWN	EA	46.4500	1,858.00
7.000	7.000	0.000	EA	1.0	CPDF112D 1-1/2 DF CLIP GALV DOM	EA	62.7000	438.90
4.000	4.000	0.000	EA	1.0	CPDF114D 1-1/4 DF CLIP GALV DOM	EA	45.9000	183.60
12.000	12.000	0.000	EA	1.0	CPDF1D 1 DF CLIP GALV DOM	EA	28.5000	342.00

**1/2" Rope**  
 \$0.18/ft x 480 total ft = \$86.40

ALL WIRE ROPE IS OF IMPORT ORIGIN UNLESS OTHERWISE SPECIFIED. ALL CLAIMS MUST BE UPON RECEIPT OF MERCHANDISE. NO UNAUTHORIZED RETURNS WILL BE ACCEPTED. NO REPRESENTATIONS OR WARRANTIES ARE MADE UNLESS SPECIFICALLY SET FORTH HERE IN WRITING. ALL RETURNS ARE SUBJECT TO OUR INSPECTION AND WE RESERVE THE RIGHT TO IMPOSE A 20% RESTOCKING CHARGE. WEST COAST WIRE ROPE ASSUMES NO RESPONSIBILITY FOR THE USE OR MISAPPLICATION OF ANY PRODUCTS SOLD BY THIS FIRM. OUR PRODUCTS ARE SOLD WITH THE EXPRESS UNDERSTANDING THAT THE PURCHASER OR USER IS THOROUGHLY FAMILIAR WITH THE CORRECT APPLICATION AND PROPER USE FOR WHICH IT IS BEING PURCHASED. WEST COAST WIRE ROPE IS NOT RESPONSIBLE FOR CUSTOMER SUPPLIED MATERIALS. WE RESERVE THE RIGHT TO IMPOSE A 1 1/2% (18% PER ANNUM) FINANCE CHARGE ON ALL PAST DUE INVOICES. FINANCE CHARGES WILL NOT BE INVOICED

\*\*\* REPRINT \*\*\*

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INVOICE	
5610665	
Invoice Date	Page
09/27/2023 14:16:32	3 of 3
ORDER NUMBER	
1743564	

Quantities					Item ID	Pricing	Unit	Extended
Ordered	Shipped	Remaining	UOM Unit Size	Disp.	Item Description	UOM Unit Size	Price	Price
4.000	4.000	0.000	EA		HST1.5T65315	EA	367.5500	1,470.20
			1.0		1.5TON CM 653 LEVER HOIST 15FT LIFT	1.0		
4.000	4.000	0.000	EA		HST.5T60310	EA	304.4500	1,217.80
			1.0		.5TON CM 603 LEVER HOIST 10 FT	1.0		

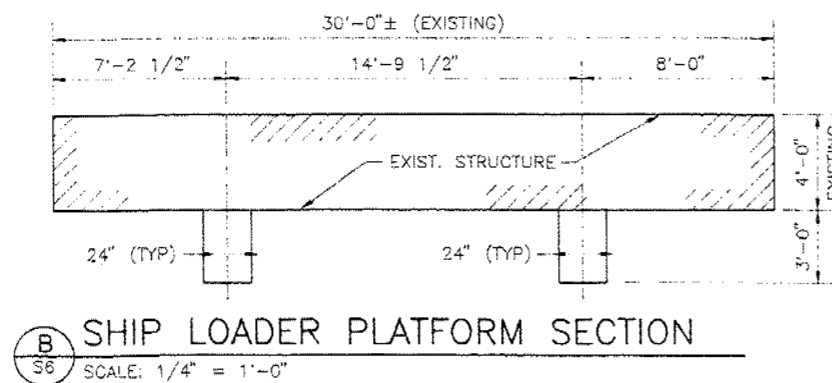
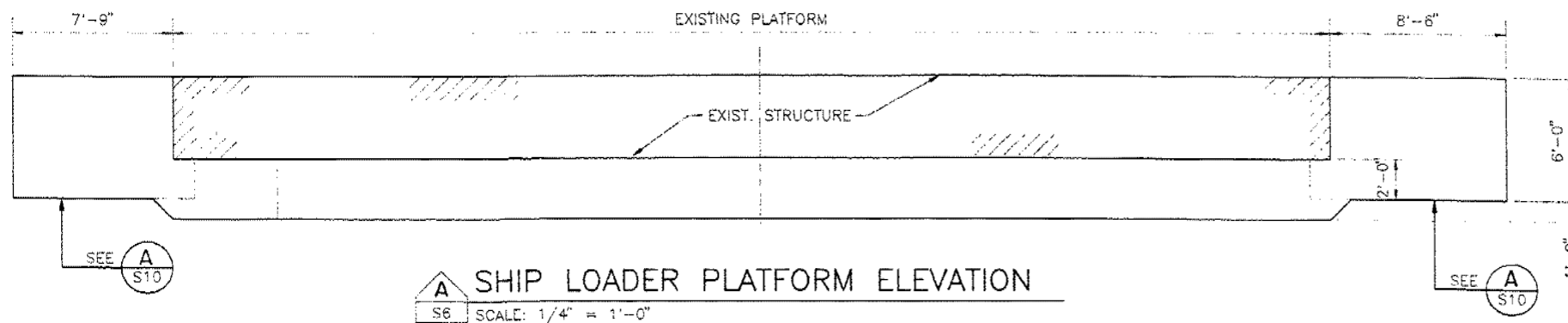
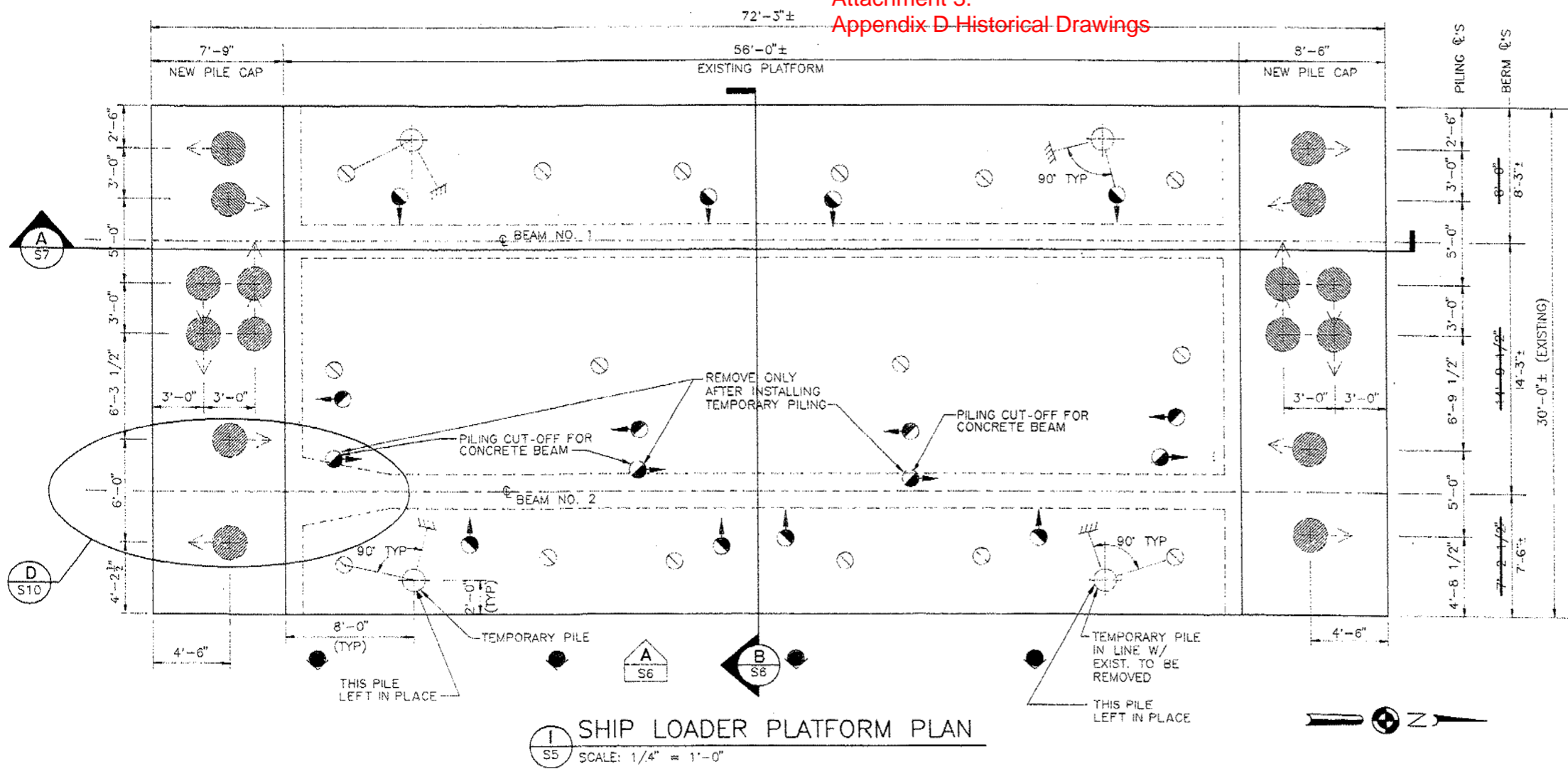
Total Lines: 20

**SUB-TOTAL:** 11,293.52  
**TAX:** 1,157.61  
**AMOUNT DUE:** 12,451.13

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\*\*\* REPRINT \*\*\*

Attachment 3:  
Appendix D Historical Drawings

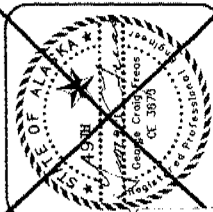


NOTES

- BATTER NORTH-SOUTH PILING 3V:1H, AND EAST-WEST PILING 4V:1H.
- BULL RAIL ON NEW CAPS NOT SHOWN.
- LOADER TOWER AND OTHER FEATURES ON EXIST. PLATFORM NOT SHOWN.
- TEMPORARY PILING:
  - TEMPORARY PILING ARE TO BE INSTALLED ONLY IF IT IS NECESSARY TO REMOVE EXISTING PILING IN ORDER TO CONSTRUCT NEW BEAMS UNDER THE SHIPLOADER PLATFORM.
  - FOR EACH PILE REMOVED PROVIDE ONE 18" DIAMETER X 0.375" WALL TEMPORARY PILE.
  - DRIVE TEMPORARY PILES TO A 30 TON WORKING CAPACITY, BUT NO LESS THAN 50 FT. PENETRATION.
  - CUT OFF TEMPORARY PILING AT AN ELEVATION SUFFICIENT TO ALLOW INSTALLATION OF 30 TON JACKS.
  - BRACE THE TOP OF TEMPORARY PILING AGAINST ADJACENT PILING OR THE UNDERSIDE OF THE LOADER PLATFORM WITH A MINIMUM OF TWO BRACES POSITIONED AT APPROXIMATELY 90° TO ONE ANOTHER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DESIGNING THE BRACES AND PROVIDING AN INSTALLATION PLAN TO THE ENGINEER.
  - WELD A MINIMUM 1" THICK PLATE TO THE TOP OF THE TEMPORARY PILING AND PLACE A BEARING PLATE BETWEEN THE 30 TON JACKS AND THE LOADER PLATFORM. THE BEARING PLATE SHALL BE 1" THICK AND HAVE A BEARING AREA OF NOT LESS THAN 1" SQUARE FOOT.
  - LOAD AND MAINTAIN EACH OF THE JACKS AT 20 TONS. LOAD THE JACKS SEQUENTIALLY IN 4 TON INCREMENTS.
  - THE CONTRACTOR MAY INSTALL ADDITIONAL TEMPORARY PILING.
  - REMOVE TEMPORARY PILING AND BRACING FOLLOWING POST-TENSIONING OF BEAMS NOL. 1 AND NO. 2.
  - TEMPORARY PILING NEED NOT BE GALVANIZED.
- DO NOT REMOVE FORMS OR SHORING FOR BEAMS NO. 1 AND NO. 2 UNTIL CONCRETE REACHES SUFFICIENT STRENGTH TO TRANSFER WEIGHT OF BEAMS TO #7 DOWELS IN VENT HOLES.

DESIGN	GCF
DRAWN	NRY
CHECK	DHA
APPROVED	GCF

**R&M ENGINEERING, INC.**  
8205 GLACIER HIGHWAY  
P.O. BOX 34278  
JUNEAU, ALASKA 99803  
PH: (907) 780-8080



SOT03-1991-AB-S6, Loader Platform -  
Plan & Sections

ALASKA INDUSTRIAL DEVELOPMENT & EXPORT AUTHORITY  
SKAGWAY ORE TERMINAL STRUCTURAL REPAIRS  
SKAGWAY, ALASKA

**LOADER PLATFORM  
PLAN & SECTIONS**



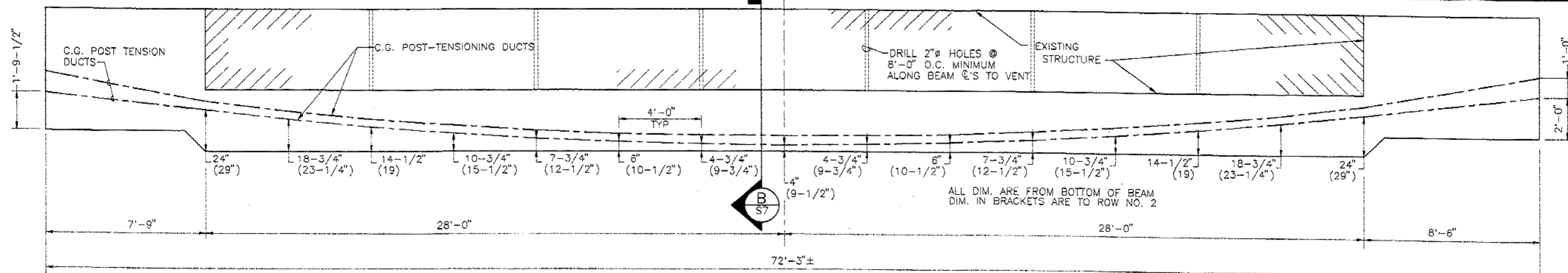
**AS-BUILT**



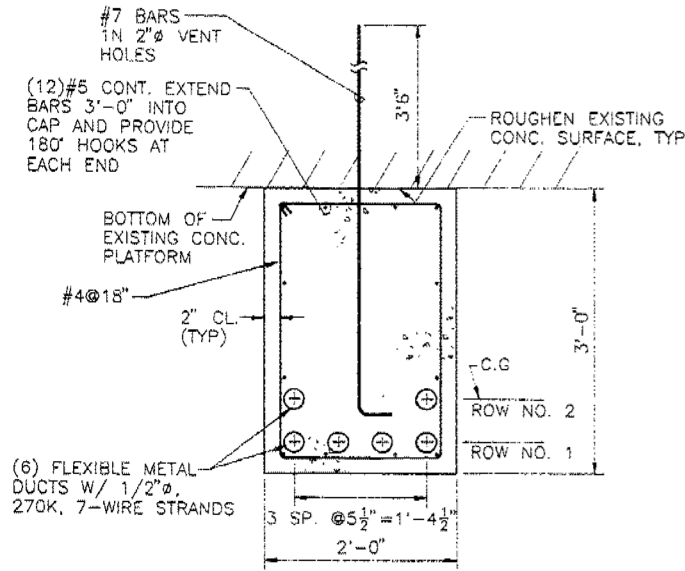
DATE: DEC. 6, 1991  
R&M NO. 901362.03  
SCALE: AS NOTED

**S6**  
SHEET  
6 of 18

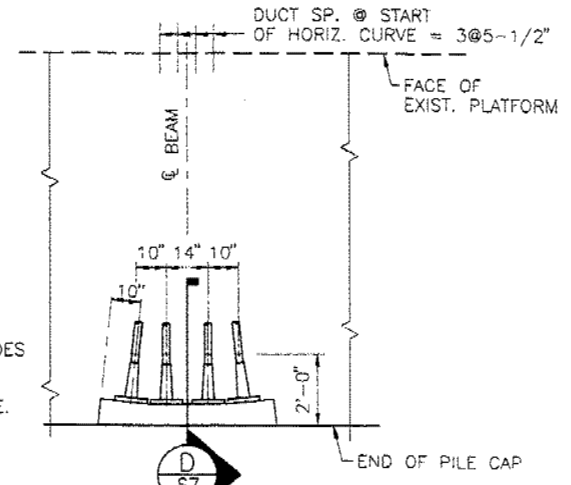
E: 05189-S6, 1=48, 12/06/91 at 14:35



**A SHIP LOADER PLATFORM POST-TENSIONING PROFILES**  
 SCALE: 3/8" = 1'-0" (BEAM NO. 1, BEAM NO. 2 SIMILAR)

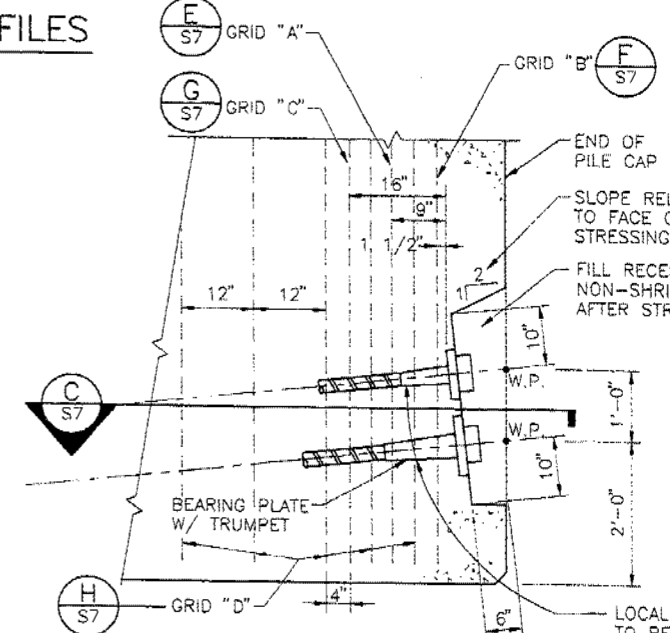


**B BEAM SECTION**  
 SCALE: 1" = 1'-0"

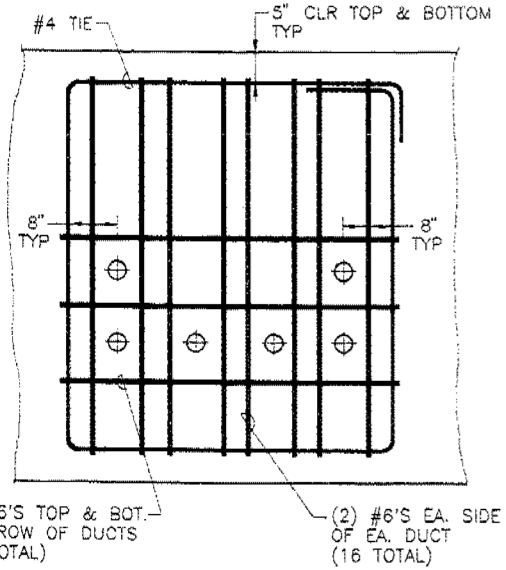


NOTE: FLAIR SIDES OF RECESS 1:3 RELATIVE TO STRESSING FACE.

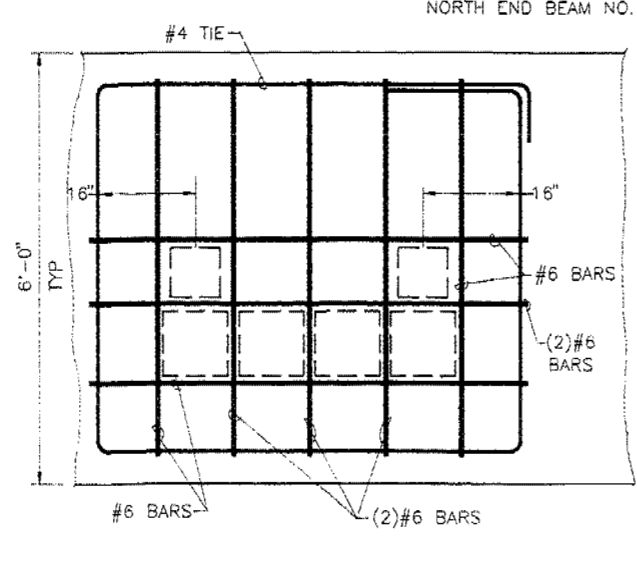
**C RECESS FOR POST-TEN. ANCHORAGES - PLAN**  
 SCALE: 3/8" = 1'-0"  
 3 REQUIRED NORTH & SOUTH ENDS BEAM NO. 1 NORTH END BEAM NO. 2



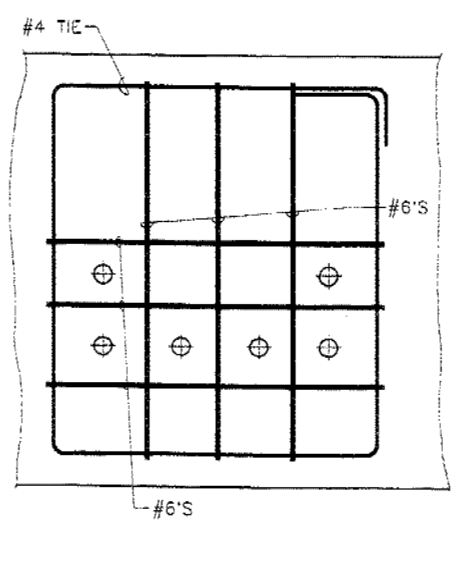
**D RECESS FOR STRESSING ANCHORAGE - NORTH END'S**  
 SCALE: 3/4" = 1'-0" (SOUTH END BEAM NO. 1 SIMILAR)



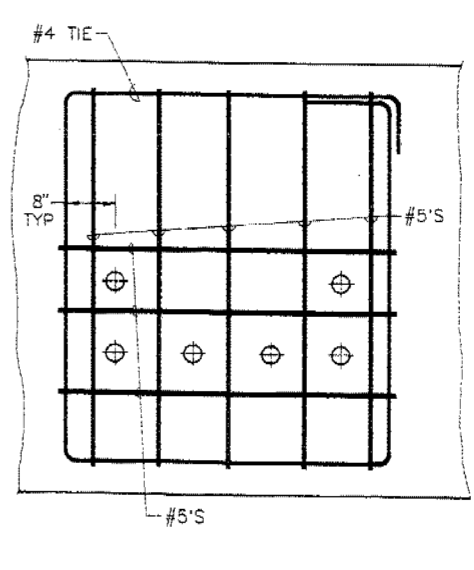
**E GRID "A"**  
 SCALE: 3/4" = 1'-0"



**F GRID "B"**  
 SCALE: 3/4" = 1'-0"

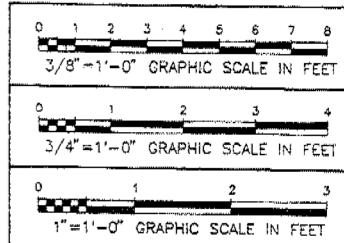


**G GRID "C"**  
 SCALE: 3/4" = 1'-0"



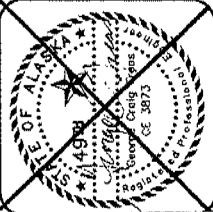
**H GRID "D"**  
 SCALE: 3/4" = 1'-0"

NOTE: PROVIDE 180° HOOKS ON BOTH ENDS OF #5 & #6 BARS IN GRIDS "A" THRU "D".



DESIGN GCF  
 DRAWN NRY  
 CHECK DHA  
 APPROVED GCF

**R&M ENGINEERING, INC.**  
 9205 GLACIER HIGHWAY  
 P.O. BOX 34278  
 JUNEAU, ALASKA 99803  
 PH: (907) 780-6050



SOT03-1991-AB-S7, Loader Platform - Details

ALASKA INDUSTRIAL DEVELOPMENT & EXPORT AUTHORITY  
 SKAGWAY ORE TERMINAL STRUCTURAL REPAIRS  
 SKAGWAY, ALASKA

**LOADER PLATFORM DETAILS**

DATE: DEC. 6, 1991  
 R&M NO. 901382.03  
 SCALE: AS NOTED

**S7**  
 SHEET  
 7 OF 18

E:05169-10, 1=32, 12/06/91 at 11:05

**Attachment 4:  
Pacific CM IDR 3/27/24**

**Skagway Ore Peninsula Redevelopment Inspector Daily Report (IDR)**

Skagway Ore Peninsula Redevelopment		KPFF Job # 2100135		
Inspector:	William Hammac Pacific CM	Date: 03.27.2024	Day: Wednesday	
Work time(s) Contractor:	<i>Pacific Pile and Marine</i>	Start: 7:00 am	Stop: 5:00 pm	
Work time(s) Contractor:	<i>PPM Broadway Yard</i>	Start: 7:00 am	Stop: 6:00 pm	
Work time(s) Contractor:	<i>PPM Redemption</i>	Start: 6:00 am	Stop: 6:00 pm	
Work time(s) Contractor:	<i>PPM Pacific Lifter</i>	Start: 6:00 am	Stop: 6:00 pm	
Work time(s) Contractor:	Cruise Dock Trestle	Start: 6:00 am	Stop: 6:00 pm	
Work time(s) Subcontractor	<i>DAMA Industrial</i>	Start: 7:00 am	Stop: 3:00 pm	
Work Time(s) Inspections: Pacific CM William Hammac		Start: 6:00 am	Stop: 6:00 pm	
Weather: 33-51°F AM: Clear PM: Clear		Precipitation: 0"		
<u>Inspector notes:</u>				
<p>PPM <i>Pacific Lifter</i> and <i>Edna Bay</i> were moored at Dolphin 5. The crew continued to demolish Dolphin 5 in preparation for installing temporary template and Fender Piles. The template was completed and Fender piles 109, 110, and 111 were installed and vibratory impacted to the required tip elevation. The requirement for impact driving was waived by KPFF and Haley Aldrich.</p> <p>PPM <i>Redemption</i> and <i>Kumtux</i> were moored at Dolphin 6. The crew started to impact Catwalk Piles 102 and 103 but Marine Mammals were spotted just when the crew started the soft start. The crew also continued to install stiffener plates as detailed in RFI 149. An "all clear" was issued at 12:05 pm and the piles were driven to the required tip elevation. This tip elevation had been changed from the -85' as indicated on Drawing S9.00, RFI 009, to -105'.</p> <p>Cruise Dock Trestle: The crew continued to weld the pile cap connections.</p> <p>Broadway Yard: A 2-man crew resumed breaking the concrete sections of the Ore Loader Platform. They have completed breaking 2 of the sections and started on a third one. They also have worked on cleaning the yard and prepared to cut temporary 24" piles into smaller sections for removal from the yard. Temporary 24" piles were cut into 40' lengths and moved to the north end of the laydown yard. The spacing of the previously installed grout tubes were confirmed to be at 8' spacing and reinforced with No. 7 epoxy coated reinforcing steel.</p> <p>Comments:</p> <p>██████████ with Haley Aldrich was onsite to monitor pile driving operations.</p> <p>██████████ with QASI was onsite to monitor welding operations.</p>				
Dolphin 5 Fender Piles				
<b>Pile #</b>	<b>Start time</b>	<b>Stop Time</b>	<b>Vibe</b>	<b>Depth</b>
109	3:30 pm	4:02 pm	32 Minutes	-134.7
110	4:35 pm	5:02 pm	23 Minutes	-135.2
111	5: 23 pm	5: pm	16 Minutes	-132.2
Vibratory times are approximate.				
Catwalk Piles				
<b>Pile #</b>	<b>Start time</b>	<b>Stop Time</b>	<b>Impact</b>	<b>Depth</b>
102	12:06 pm	12:15 pm	9 Minutes	-109.2'
103	12:25 pm	12:36 pm	11 Minutes	-108.6'
Final tip elevation requirements had been changed from the -85.0' elevation as indicated on Drawing S9.00, RFI 009.				
The Marine Mammal Observers were onsite and issued an "all clear" for impact driving at 6:45 am. The Redemption crew conducted a soft start on Pile 102 then an "all stop" was called at 6:58 am. An "all clear" for impact driving was also re-issued at 12:05 pm and impact driving operations resumed.				
<i>Pacific Pile and Marine (prime)</i>	Craft/Position	Contract Work (Hrs)	Change order #	
<input type="checkbox"/> Daily job hazard analysis (JHA) completed:				

<input type="checkbox"/>		Superintendent (206)702-3679	12	
<input checked="" type="checkbox"/>		Deputy Project Manager	10	
<input checked="" type="checkbox"/>		PM	10	
<input checked="" type="checkbox"/>		Field Engineer	Off Site	
<input checked="" type="checkbox"/>		Field Engineer	Off Site	
<input checked="" type="checkbox"/>		Field Engineer	10	
<input checked="" type="checkbox"/>		Edge Survey and Design	10	

<i>Pacific Pile and Marine (prime)</i>	Redemption Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Welder	12	
<input checked="" type="checkbox"/>	Welder	12	
<input checked="" type="checkbox"/>	Welder	12	
<input checked="" type="checkbox"/>	Pile Buck	0	
<input checked="" type="checkbox"/>	Foreman	0	
<input checked="" type="checkbox"/>	Welder	0	
<input checked="" type="checkbox"/>	Operator	0	
<input checked="" type="checkbox"/>	Crane Operator	12	

<i>Pacific Pile and Marine (prime)</i>	Lifter Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Crane Operator	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>			
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Operator	12	
<input checked="" type="checkbox"/>	Pile Buck	12	

<i>Pacific Pile and Marine (prime)</i>	Cruise Dock Trestle Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Operator	12	Crane Maintenance
<input checked="" type="checkbox"/>	Pile buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	Crane Maintenance.
<input checked="" type="checkbox"/>			

<b>Subcontractor #1</b>			
<b>DAMA</b>			
<input checked="" type="checkbox"/> Daily job hazard analysis (JHA) completed:			
<input checked="" type="checkbox"/>	Journeyman	8	
<input checked="" type="checkbox"/>	Labor	8	

	Subject	Comments
7:00 am	PPM Lifter and Edna Bay Dolphins 4 and 5.	The crew resumed work on Dolphin 5.
	PPM Redemption and Kumtux	The crew started to impact Catwalk Pile 102 but were shut down at 6:58 am due to marine mammals in the exclusion zone.



	<p>Cruise Dock Trestle</p> <p><i>Broadway Yard</i></p> <p><i>Coating</i></p>	<p>A 4-man crew continued welding pile the pile caps on Grid Line D and E.</p> <p>The crew continued to demolish the Ore Loader Concrete Platform sections. They have also worked on cleaning the yard.</p> <p>DAMA set up and began to repair the coating on Dolphin 7.</p>
9:00 am	<p><i>PPM Lifter and Edna Bay were at the Cruise Dock Float Guide.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p>Cruise Dock Trestle</p> <p><i>Broadway Yard</i></p> <p><i>Coating</i></p>	<p>The crew set the template on Dolphin 5.</p> <p>The crew continued to be on "stand-by" at Catwalk No. 1.</p> <p>A 2-man crew continued welding pile the pile caps on Grid Line D and E. 2 other crewmembers work on the Hamilton crane.</p> <p>A 2-man crew resumed demolition of the Ore Loader Platform.</p> <p>DAMA continued to do coating repairs on Dolphin 7.</p>
12:00 pm	<p><i>PPM Lifter and Edna Bay were at the Cruise Dock Float Guide.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p>Cruise Dock Trestle</p> <p><i>Broadway Yard</i></p> <p><i>Coating</i></p>	<p>The crew continued working on the template for Dolphin 5.</p> <p>The "all clear" was issued by the Marine Mammal Observers and the crew drove piles 102 and 103 with the impact to the required elevation.</p> <p>A 4-man crew continued welding pile the pile caps on Grid Line C, D, and E.</p> <p>A 2-man crew resumed demolition of the Ore Loader Platform.</p> <p>DAMA had completed coating of what was available for Dolphin 7.</p>
3:00 pm	<p><i>PPM Lifter and Edna Bay relocated to Dolphin 5.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p>Cruise Dock Trestle</p> <p><i>Broadway Yard</i></p> <p><i>Coating</i></p>	<p>The crew began to vibratory impact piles 109, 110, and 111 Dolphin 5 Fender Piles to the required tip elevation. Impact driving of these piles was waived by Haley Aldrich and KPFF.</p> <p>The crew continued to work on the stiffeners for Dolphin 6.</p> <p>A 4-man crew continued welding pile the pile caps on Grid Line C, D, and E.</p> <p>A 2-man crew resumed demolition of the Ore Loader Platform.</p> <p>DAMA had completed what was available at Dolphin 7.</p>
5:00 pm	<p><i>PPM Lifter and Edna Bay at Dolphin 5.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p>Cruise Dock Trestle</p> <p><i>Broadway Yard</i></p>	<p>The crew completed driving piles 109, 110, and 111 for the Dolphin 5 Fender Piles at 5:45 pm.</p> <p>The crew continued to work on the stiffeners for Dolphin 6.</p> <p>A 4-man crew continued welding pile the pile caps on Grid Line C, D, and E.</p> <p>A 2-man crew resumed demolition of the Ore Loader Platform.</p>

Project site variables (Skagway tide, sunrise, and wind data)						
High Tide	2:56 am	17.28ft.		Sunrise	6:39 am	
Low Tide	9:13 am	-0.76		Sunset	7:34 pm	
High Tide	3:22 pm	15.64 ft.		Civil Twilight Dawn	5:58 am	
Low Tide	9:19 pm	1.51 ft.		Civil Twilight Dusk	8:15 pm	

Wind Speed by Time	Sustained (mph/kts)	Gusting to(mph/kts)
7:00 am	3/2 N	4/3 NW
8:30 am	4/3 N	6/5 NNW
11:00 am	5/4 S	6/5 SSW
1:00 pm	10/9 N	18/16 N

Environmental/ TESC/SWPPP		
Environmental issues for today:		
Containment curtain boom is not in place and is available near the ore containment railroad.	BMPs in place on the Redemption Hydrocarbon absorption boom is available on the Redemption	BMPs in place on the Pacific Lifter Hydrocarbon absorption boom is available on the Pacific Lifter

Safety	
Topic or concern	Description
Maring of Parking Surface	A Genie 135 lift had broken down at the Cruise Dock Trestle and the crew pulled it away from the work area with a large forklift and left tire tracks across the existing parking lot.
Material Received & Condition: None	
Discrepancies/Any Cause for Dispute or Change Order: None	
Site Visitors:	

**Daily Photographs**

	<p>Subject: PPM Lifter</p> <p>Location: Dolphins 4 and 5.</p> <p>Comment: The crew worked on setting the template for Dolphin 5.</p>
	<p>Subject: PPM Lifter</p> <p>Location: Dolphin 5</p> <p>Comment: The crew had completed the template and staged Pile 111.</p>



Subject: *PPM Redemption*

Location: Catwalk Piles

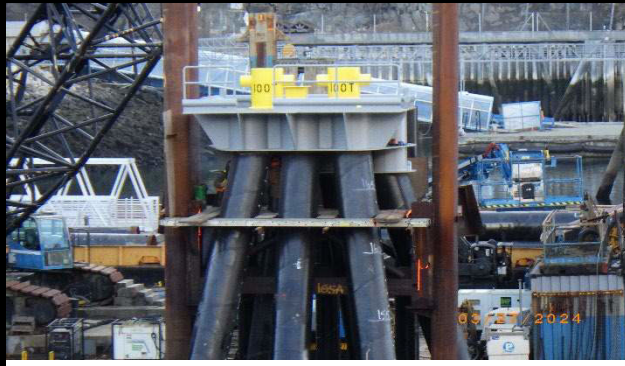
Comment: The crew set the impact hammer on Pile 102 and were delayed from 6:58 am. to 12:05 pm due to marine mammals in the exclusion zone.



Subject: Cruise Dock Trestle Piles

Location: Cruise Dock Trestle

Comment: The crew continued welding pile caps D, and E.



Subject: *PPM Redemption*

Location: Dolphin 6

Comment: The crew continued welding stiffener plates.



Subject: *PPM Redemption*

Location: Catwalk Piles

Comment: The crew received the "all clear" from the Marine Mammal Observers at 12:05 pm and drove pile 102 with the impact hammer to the required tip elevation.



Subject: Pile to Pile Cap Welds

Location: Cruise Dock Trestle

Comment: The crew continued to weld the caps for Grid Lines D, and E.



Subject: Cruise Dock Trestle Lay-Down Ara

Location: Cruise Dock Trestle

Comment: A Genie SX-135 XC lift had broken down in the work area and was dragged across the parking lot by a large forklift and left tire tracks across the existing parking area pavement.



Subject: Cruise Dock Trestle Lay-Down Ara

Location: Cruise Dock Trestle

Comment: A Genie SX-135 XC lift had broken down in the work area and was dragged across the parking lot by a large forklift and left tire tracks across the existing parking area pavement.



Subject: Coating Repair

Location: Dolphin 7

Comment: DAMA repaired damaged coating on piles.



Subject: Ore Loader Platform Concrete Sections

Location: Broadway Yard

Comment: A (2) man crew continued to break the concrete sections of the Ore Loader Platform.



Subject: Ore Loader Platform Concrete Sections

Location: Broadway Yard

Comment: It was confirmed that the previously installed grout tubes from the top section of the platform into the post-tension beams were reinforced with epoxy coated No. 7 reinforcing steel at approximately 8' on center.



Subject: Ore Loader Platform Concrete Sections

Location: Broadway Yard

Comment: It was noted that a reinforced grout tube had been previously installed horizontally across the lower section of the Ore Loader Foundation.



Subject: PPM Lifter

Location: Dolphin 5

Comment: The crew had driven Pile 111 and set Pile 110.



Subject: PPM Lifter

Location: Dolphin 5

Comment: The crew drove Pile 109.

*William Hammac* William Hammac 03.27.2024

Inspector Signature Name Date

**Attachment 5:  
Pacific CM IDR 3/28/24**

**Skagway Ore Peninsula Redevelopment Inspector Daily Report (IDR)**

Skagway Ore Peninsula Redevelopment		KPFF Job # 2100135																					
Inspector:	William Hammac Pacific CM	Date: 03.28.2024	Day: Thursday																				
Work time(s) Contractor:	<i>Pacific Pile and Marine</i>	Start: 7:00 am	Stop: 5:00 pm																				
Work time(s) Contractor:	<i>PPM Broadway Yard</i>	Start: 7:00 am	Stop: 6:00 pm																				
Work time(s) Contractor:	<i>PPM Redemption</i>	Start: 6:00 am	Stop: 6:00 pm																				
Work time(s) Contractor:	<i>PPM Pacific Lifter</i>	Start: 6:00 am	Stop: 6:00 pm																				
Work time(s) Contractor:	Cruise Dock Trestle	Start: 6:00 am	Stop: 6:00 pm																				
Work time(s) Subcontractor	<i>DAMA Industrial</i>	Start: 7:00 am	Stop: 5:00 pm																				
Work Time(s) Inspections: Pacific CM William Hammac		Start: 6:00 am	Stop: 6:00 pm																				
Weather: 33-53°F AM: Clear PM: Clear		Precipitation: 0"																					
<u>Inspector notes:</u>																							
<p>PPM <i>Pacific Lifter</i> and <i>Edna Bay</i> were moored at Dolphin 5. The crew began to prepare to install the fender panel hardware. The crew completed this task and installed the Fender Panel.</p> <p>PPM <i>Redemption</i> and <i>Kumtux</i> were moored at Dolphin 6. The crew cut Catwalk Piles 102 and 103 to the required cut-off elevation. The crew had also completed the installation of the I stiffener plates as detailed in RFI 149. The welds were checked by the QASI inspector and were approved for coating. DAMA set up containment and sandblasted the pile to cap connections.</p> <p>Cruise Dock Trestle: The crew continued to weld the pile cap connections. The welds were completed and the crew set bearing pads and precast concrete panels for Grid Line A-1 to A-1 and B-1 to B-2.</p> <p>Broadway Yard: A 2-man crew continued breaking the concrete sections of the Ore Loader Platform. The spacing of two of the previously installed grout tubes were measured at 12'6". They also removed equipment, pile cut-offs, and a storage container away from the white trailer and the old ramp.</p> <p>DAMA sandblasted and coated Dolphin 6.</p> <p>Comments:  <span style="background-color: black; color: black;">██████████</span> with Haley Aldrich was onsite to monitor pile driving operations.  <span style="background-color: black; color: black;">██████████</span> with QASI was onsite to monitor welding operations.</p> <p>Dolphin 4 Fender Piles</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 10%;">Pile #</th> <th style="width: 15%;">Start time</th> <th style="width: 15%;">Stop Time</th> <th style="width: 15%;">Vibe</th> <th style="width: 45%;">Depth</th> </tr> </thead> <tbody> <tr> <td>106</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>107</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>108</td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table> <p>Vibratory times are approximate.</p>				Pile #	Start time	Stop Time	Vibe	Depth	106					107					108				
Pile #	Start time	Stop Time	Vibe	Depth																			
106																							
107																							
108																							
<i>Pacific Pile and Marine (prime)</i>		Craft/Position	Contract Work (Hrs)	Change order #																			
<input type="checkbox"/> Daily job hazard analysis (JHA) completed:																							

<input type="checkbox"/>		Superintendent (206)702-3679	12	
<input checked="" type="checkbox"/>		Deputy Project Manager	10	
<input checked="" type="checkbox"/>		PM	10	
<input checked="" type="checkbox"/>		Field Engineer	Off Site	
<input checked="" type="checkbox"/>		Field Engineer	Off Site	
<input checked="" type="checkbox"/>		Field Engineer	10	
<input checked="" type="checkbox"/>		Edge Survey and Design	10	

<i>Pacific Pile and Marine (prime)</i>	Redemption Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Welder	12	
<input checked="" type="checkbox"/>	Welder	12	
<input checked="" type="checkbox"/>			
<input checked="" type="checkbox"/>	Pile Buck	0	
<input checked="" type="checkbox"/>	Foreman	0	
<input checked="" type="checkbox"/>	Welder	0	
<input checked="" type="checkbox"/>	Operator	0	
<input checked="" type="checkbox"/>	Crane Operator	12	

<i>Pacific Pile and Marine (prime)</i>	Lifter Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Crane Operator	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>			
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Operator	12	
<input checked="" type="checkbox"/>	Pile Buck	12	

<i>Pacific Pile and Marine (prime)</i>	Cruise Dock Trestle Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>	Foreman	12	
<input checked="" type="checkbox"/>	Operator	12	
<input checked="" type="checkbox"/>	Pile buck	12	
<input checked="" type="checkbox"/>	Pile Buck	12	
<input checked="" type="checkbox"/>	Pile Buck	6	

**Subcontractor #1**

**DAMA**

Daily job hazard analysis (JHA) completed:

<input checked="" type="checkbox"/>	Journeyman	10	
<input checked="" type="checkbox"/>	Labor	10	

	Subject	Comments
8:00 am	PPM Lifter and Edna Bay Dolphins 4 and 5. PPM Redemption and Kumtux Cruise Dock Trestle	The crew resumed work on Dolphin 5. The crew cut Catwalk Piles 102 and 103 at the required cut-off elevation and prepared to set the cap.

	<p><i>Broadway Yard</i></p> <p><i>DAMA Industrial</i></p>	<p>A 4-man crew continued welding pile the pile caps on Grid Line D and E.</p> <p>The crew continued to demolish the Ore Loader Concrete Platform sections. They have also worked on cleaning the yard.</p> <p>DAMA set up and began to repair the coating on Dolphin 7.</p>
9:00 am	<p><i>PPM Lifter and Edna Bay were at the Cruise Dock Float Guide.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p><i>Cruise Dock Trestle</i></p> <p><i>Broadway Yard</i></p> <p><i>DAMA Industrial</i></p>	<p>The crew set the template on Dolphin 5.</p> <p>The crew had completed prep work to set the cap for Catwalk No. 2.</p> <p>A 2-man crew continued welding pile the pile caps on Grid Line D and E. 2 other crewmembers work on the Hamilton crane.</p> <p>A 2-man crew resumed demolition of the Ore Loader Platform.</p> <p>DAMA continued to do coating repairs on Dolphin 7.</p>
12:00 pm	<p><i>PPM Lifter and Edna Bay were at the Cruise Dock Float Guide.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p><i>Cruise Dock Trestle</i></p> <p><i>Broadway Yard</i></p> <p><i>DAMA Industrial</i></p>	<p>The crew had removed the temporary template from Dolphin 5.</p> <p>The crew set the cap for Catwalk No. 2.</p> <p>A crew member continued welding a pile to cap weld.</p> <p>The crew continued to clean up the Broadway Yard and moved a connex and other equipment away from the Port X-ray trailer.</p> <p>The crew sandblasted the underside of Dolphin 6.</p>
2:00 pm	<p><i>PPM Lifter and Edna Bay relocated to Dolphin 5.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p><i>Cruise Dock Trestle</i></p> <p><i>Broadway Yard</i></p> <p><i>DAMA Industrial</i></p>	<p>The crew continued to install the fender panel on Dolphin 5.</p> <p>The crew continued welding the cap for Catwalk No. 2.</p> <p>The crew re-arranged precast panels and set Panel PC4 on Grid Lines A-1 to B-1. Another crew member joined the team and the surveyor assisted.</p> <p>The crew continued to clean the yard and break the concrete sections of the Ore Loader Platform.</p> <p>The crew had completed sandblasting of Dolphin 6 and began to coat the piles and cap.</p>
4:00 pm	<p><i>PPM Lifter and Edna Bay at Dolphin 5.</i></p> <p><i>PPM Redemption and Kumtux</i></p> <p><i>Cruise Dock Trestle</i></p> <p><i>Broadway Yard</i></p> <p><i>DAMA Industrial</i></p>	<p>The crew moved anchors, etc.</p> <p>The crew completed setting the pile cap for Catwalk 2.</p> <p>The crew set (3) PC3 and (1) PC2 precast panels on Grid A to Grid B.</p> <p>The crew continued to clean the yard and break the concrete sections of the Ore Loader Platform</p> <p>DAMA completed coating Dolphin 6.</p>

Project site variables (Skagway tide, sunrise, and wind data)					
High Tide	3:22 am	17.17 ft.		Sunrise	6:36 am
Low Tide	9:45 am	-0.61 ft.		Sunset	7:36 pm
High Tide	3:55 pm	14.9 ft.		Civil Twilight Dawn	5:55 am
Low Tide	9:48 pm	2.59 ft.		Civil Twilight Dusk	8:18 pm
Wind Speed by Time	Sustained (mph/kts)	Gusting to(mph/kts)			





7:00 am	3/2 N	4/3 NW
10:00 am	8/7 N	17/15 NNW
12:00 pm	9/8 N	17/15 NNW
3:00 pm	5/4 N	10/9 N

Environmental/ TESC/SWPPP		
Environmental issues for today:		
Containment curtain boom is not in place and is available near the ore containment railroad.	BMPs in place on the Redemption Hydrocarbon absorption boom is available on the Redemption	BMPs in place on the Pacific Lifter Hydrocarbon absorption boom is available on the Pacific Lifter

Safety	
Topic or concern	Description
Material Received & Condition: None	
Discrepancies/Any Cause for Dispute or Change Order: None	
Site Visitors:	

**Daily Photographs**

	<p>Subject: <i>PPM Lifter</i></p> <p>Location: Dolphins 4 and 5.</p> <p>Comment: The crew had removed the temporary template from Dolphin 5.</p>
	<p>Subject: <i>PPM Lifter</i></p> <p>Location: Dolphin 5</p> <p>Comment: The crew had cut the Fender Piles to the cut-off elevation.</p>



Subject: *PPM Redemption*

Location: Catwalk Piles

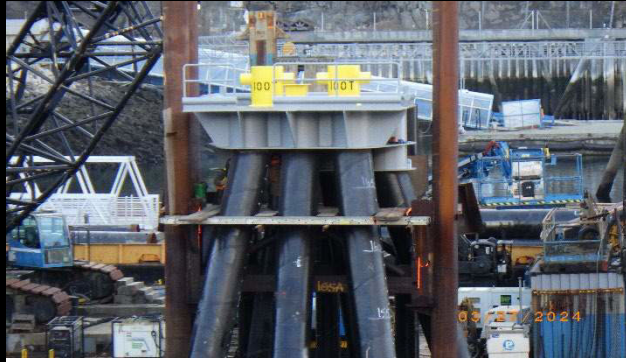
Comment: The crew had cut piles 102 and 103 at the cut-off elevation.



Subject: Cruise Dock Trestle Piles

Location: Cruise Dock Trestle

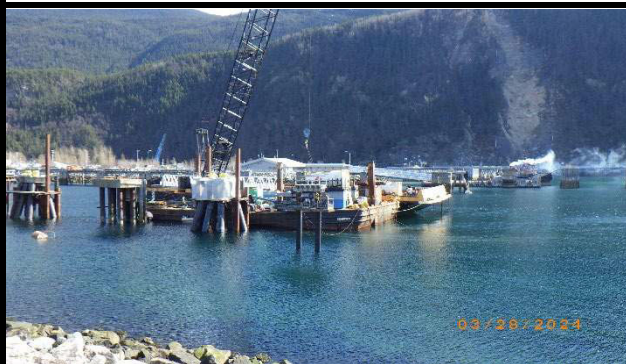
Comment: The crew continued welding pile caps D, and E.



Subject: *PPM Redemption*

Location: Dolphin 6

Comment: The crew had completed welding stiffener plates.



Subject: *PPM Redemption*

Location: Catwalk No. 2

Comment: The crew set the cap for Catwalk No. 2.



Subject: Pile to Pile Cap Welds

Location: Cruise Dock Trestle

Comment: The crew staged precast panels closer to the trestle area.



Subject: Cruise Dock Trestle Lay-Down Area

Location: Cruise Dock Trestle

Comment: Welding is almost complete on the trestle.



Subject: Cruise Dock Trestle

Location: Cruise Dock Trestle

Comment: Work platforms were installed on Grid Lines A and B for better access.



Subject: Cruise Dock Trestle

Location: Cruise Dock Trestle

Comment: The crew set Precast Panel PC4 on Grid Lines A-1 to B-1. .



Subject: Coating Repair

Location: Dolphin 6

Comment: DAMA had set up containment and sand blasted the piles and cap.



Subject: X-Ray Trailer

Location: Broadway Yard

Comment: The crew had removed (4) 24” piles and equipment away from the port x-ray trailer in the Broadway Yard.



Subject: Ore Loader Platform Concrete Sections  
 Location: Broadway Yard  
 Comment: The crew removed material and cleaned up the Broadway Yard.



Subject: Ore Loader Platform Concrete Sections  
 Location: Broadway Yard  
 Comment: Reinforced grout tube location was checked and it was discovered that the tube was installed 12.5' from the other grout tube on the beam.



Subject: PPM Lifter  
 Location: Dolphin 5  
 Comment: The crew set the Fender Panel.



Subject: PPM Lifter  
 Location: Dolphin 5  
 Comment: The crew set the Fender Panel.

*William Hammac* William Hammac 03.28.2024

Inspector Signature Name Date

**Attachment 6:  
Pacific CM IDR 4/6/24**

**Skagway Ore Peninsula Redevelopment Inspector Daily Report (IDR)**

Skagway Ore Peninsula Redevelopment		KPFF Job # 2100135																	
Inspector:	William Hammac Pacific CM	Date: 04.06.2024	Day: Saturday																
Work time(s) Contractor:	<i>Pacific Pile and Marine</i>	Start: 7:00 am	Stop: 5:00 pm																
Work time(s) Contractor:	<i>PPM Broadway Yard</i>	Start: N/A	Stop: N/A																
Work time(s) Contractor:	<i>PPM Redemption</i>	Start: 6:00 am	Stop: 6:00 pm																
Work time(s) Contractor:	<i>PPM Pacific Lifter</i>	Start: 6:00 am	Stop: 6:00 pm																
Work time(s) Contractor:	Cruise Dock Trestle	Start: 6:00 am	Stop: 6:00 pm																
Work time(s) Subcontractor	<i>DAMA Industrial</i>	Start: 7:00 am	Stop: 6:00 pm																
Work Time(s) Inspections: Pacific CM William Hammac		Start: 6:00 am	Stop: 6:00 pm																
Weather: 29-45°F AM: Mostly Cloudy PM: Partly Cloudy		Precipitation: 0.25"																	
<u>Inspector notes:</u>																			
<p>PPM <i>Pacific Lifter</i> and <i>Edna Bay</i> were moored at Dolphin 5. An enclosure was set around Dolphin 1 and work activities are not visible from the land side. Welding inspections will be conducted by a QASI welding inspector. The inspector reported that the crew had completed the root passes of the pile to cap connections. The crew also removed the temporary piles from Dolphin 2.</p> <p>PPM <i>Redemption</i> and <i>Kumtux</i> were moored at Dolphin 6. DAMA had completed sandblasting the pile to cap connection and applied a coat of primer to damaged sections of the cap as well as a coat to the pile to cap connections for Dolphin 7.</p> <p>Cruise Dock Trestle: The crew continued to cut and prep the washer plates for the precast panel to cap connections.</p> <p>Broadway Yard: Hunz and Hunz was not onsite. A PPM operator resumed demolition of the Ore Loader Platform and completed that section. Only one section remains. PPM exposed two previously installed grout tubes from the cap to the beams and it was noted that the grout tubes were approximately 10' apart and that the rebar did not penetrate into the added beam.</p> <p>Comments:  <span style="background-color: black; color: black;">██████████</span> with Haley Aldrich was not onsite to monitor pile driving operations.  <span style="background-color: black; color: black;">██████████</span> with QASI was onsite to monitor welding operations.                      The Marine Mammal Observers issued an "all clear" for vibratory removal of the temporary template piles for Dolphin 2 at 6:35 am. PPM started to remove the piles at 9:30 am due to a mechanical issue with the <i>Lifter</i>.</p> <p>Dolphin 2 Temporary Pile Extraction</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 10%;">Pile #</th> <th style="width: 15%;">Start time</th> <th style="width: 15%;">Stop Time</th> <th style="width: 60%;">Vibration</th> </tr> </thead> <tbody> <tr> <td>2A</td> <td>9:58 am</td> <td>10:10 am</td> <td>11 Minutes</td> </tr> <tr> <td>2B</td> <td>1:18 pm</td> <td>1:35 pm</td> <td>12 Minutes</td> </tr> <tr> <td>2C</td> <td>3:22 pm</td> <td>3:46 pm</td> <td>12 Minutes</td> </tr> </tbody> </table> <p>Elapsed time of vibration is approximate.</p>				Pile #	Start time	Stop Time	Vibration	2A	9:58 am	10:10 am	11 Minutes	2B	1:18 pm	1:35 pm	12 Minutes	2C	3:22 pm	3:46 pm	12 Minutes
Pile #	Start time	Stop Time	Vibration																
2A	9:58 am	10:10 am	11 Minutes																
2B	1:18 pm	1:35 pm	12 Minutes																
2C	3:22 pm	3:46 pm	12 Minutes																
<i>Pacific Pile and Marine (prime)</i>		Craft/Position	Contract Work (Hrs)																
<input type="checkbox"/> Daily job hazard analysis (JHA) completed:																			

<input type="checkbox"/>		Superintendent (206)702-3679	12	
<input checked="" type="checkbox"/>		Deputy Project Manager	10	
<input checked="" type="checkbox"/>		PM	10	
<input checked="" type="checkbox"/>		Field Engineer	10	
<input checked="" type="checkbox"/>		Field Engineer	0	
<input checked="" type="checkbox"/>		Field Engineer	0	
<input type="checkbox"/>		Edge Survey and Design	0	

<i>Pacific Pile and Marine (prime)</i>		Redemption Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>		Foreman	12	
<input checked="" type="checkbox"/>		Welder	12	
<input checked="" type="checkbox"/>		Welder	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Pile Buck	0	
<input checked="" type="checkbox"/>		Foreman	0	
<input checked="" type="checkbox"/>		Welder	0	
<input checked="" type="checkbox"/>		Operator	0	
<input checked="" type="checkbox"/>		Crane Operator	12	

<i>Pacific Pile and Marine (prime)</i>		Lifter Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>		Foreman	12	
<input checked="" type="checkbox"/>		Crane Operator	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>				
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Operator	12	
<input checked="" type="checkbox"/>		Pile Buck	12	

<i>Pacific Pile and Marine (prime)</i>		Cruise Dock Trestle Crew Craft/Position	Contract Work (Hrs)	Change order #
<input checked="" type="checkbox"/>		Foreman	12	
<input checked="" type="checkbox"/>		Operator	12	
<input checked="" type="checkbox"/>		Pile buck	12	
<input checked="" type="checkbox"/>		Pile Buck	12	
<input checked="" type="checkbox"/>		Pile Buck		

**Subcontractor #1**  
**DAMA**

Daily job hazard analysis (JHA) completed:

<input type="checkbox"/>		Journeyman		
<input type="checkbox"/>		Labor		
<input checked="" type="checkbox"/>		Project Manager	8	
<input checked="" type="checkbox"/>		Journeyman	8	

**Subcontractor #2**  
**Hunz and Hunz**

Daily job hazard analysis (JHA) completed:

<input checked="" type="checkbox"/>		Journeyman	0	
<input checked="" type="checkbox"/>		Labor	0	
<input checked="" type="checkbox"/>		Train Conductor	0	

	Subject	Comments
7:00 am	PPM Lifter and Edna Bay were moored at Dolphins 4 and 5.  PPM Redemption and Kumtux  Cruise Dock Trestle	The crew mobilized to Dolphin 1.  DAMA had applied a coat to the pile to cap connections as well as primer to other damaged areas of the cap.

	<i>Broadway Yard</i>	The crew continued to measure, cut, drill bolt holes, and prepare washer plates for the panel to cap connections. Hunz and Hunz was not onsite.
8:00 am	<i>PPM Lifter and Edna Bay were moored at Dolphins 4 and 5.</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew had completed the root passes of the pile to cap connection and these welds were checked by the QASI welding inspector.  The crew assisted DAMA at Dolphin 7.  The crew was working on the washer plates. These activities took place inside of the shipping container.  No Activity
9:30 am	<i>PPM Lifter and Edna Bay were moored at Dolphin 4</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew continued to weld the pile to cap connections at Dolphin 1. The crew also set up to extract pile 2A from Dolphin 2 template. The pile was clear of the template at 10:10 am.  The crew assisted DAMA with operations at Dolphin 7.  The crew was working on the washer plates. These activities took place inside of the shipping container.  An operator resumed work on the Ore Loader Platform concrete demolition.
1:00 pm	<i>PPM Lifter and Edna Bay were moored at Dolphin 4.</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew continued to weld the pile to cap connections at Dolphin 1. The crew also set up to extract pile 2B from Dolphin 2 template. The pile was clear of the template at 1:40 pm.  DAMA had completed coating of Dolphin 7.  The crew was working on the washer plates. These activities took place inside of the shipping container. The crew also set panel PC1 from Grid A-3 to B-3. DAMA also applied touch up coating as required.  An operator continued to demolish the sections of the Ore Loader Platform.
3:00 pm	<i>PPM Lifter and Edna Bay were moored at Dolphin 4.</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew continued to weld the pile to cap connections at Dolphin 1. The crew also extracted pile 2C from Dolphin 2.  No Activity  The crew was working on the washer plates. These activities took place inside of the shipping container.  An operator continued to demolish the sections of the Ore Loader Platform. PPM exposed two previously installed grout tubes from the cap to the beams and it was noted that the grout tubes were approximately 10' apart and that the rebar did not penetrate into the added beam.
4:00 pm	<i>PPM Lifter and Edna Bay were moored at Dolphin 4.</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew continued to weld the pile to cap connections at Dolphin 1.  No Activity  The crew was working on the washer plates. These activities took place inside of the shipping container.  An operator had completed demolition of one section of the Ore Loader Platform.
5:00 pm	<i>PPM Lifter and Edna Bay were moored at Dolphin 4.</i>  <i>PPM Redemption and Kumtux</i>  Cruise Dock Trestle  <i>Broadway Yard</i>	The crew continued to weld the pile to cap connections at Dolphin 1. They also completed removing the temporary piles from Dolphin 2.  No Activity.  The crew was working on the washer plates. These activities took place inside of the shipping container.  No Activity



Project site variables (Skagway tide, sunrise, and wind data)						
High Tide	12:07 am	16.03 ft.		Sunrise	6:10 am	
Low Tide	6:07 am	0.86 ft.		Sunset	7:58 pm	
High Tide	12:08 pm	17.09 ft.		Civil Twilight Dawn	5:27 am	
Low Tide	6:26 pm	-1.62 ft.		Civil Twilight Dusk	8:41 pm	

Wind Speed by Time	Sustained (mph/kts)	Gusting to(mph/kts)
6:30 am	5/4 SSW	6/5 SSW
8:30 am	5/4 SSW	10/9 SSW
10:00 am	11/10 SSE	20/18 SSE
1:00 pm	19/17 SSE	26/23 SSE
5:00 pm	23/20 SSE	30/26 SSE

Environmental/ TESC/SWPPP		
Environmental issues for today:		
Containment curtain boom is not in place and is available near the ore containment railroad.	BMPs in place on the Redemption Hydrocarbon absorption boom is available on the Redemption	BMPs in place on the Pacific Lifter Hydrocarbon absorption boom is available on the Pacific Lifter

Safety	
Topic or concern	Description
Fuel Line	The Zidell Marine 277 fuel barge moored at the Broadway Dock and Petro Marine began the offload had had an issue with fuel flow through the pipes. They were assured that no one with the project team had touched any of the valves. This was corrected in the Petro Marine tank farm and the barge was unloaded.
Material Received & Condition: None	
Discrepancies/Any Cause for Dispute or Change Order: None	
Site Visitors: with KPFF was on the project site.	

**Daily Photographs**

	<p>Subject: PPM Lifter</p> <p>Location: Dolphin 1</p> <p>Comment: The crew welded the root pass for the pile to cap connections.</p>
	<p>Subject: PPM Lifter</p> <p>Location: Dolphin 2</p> <p>Comment: The crew extracted Temporary Pile 2A.</p>





Subject: *PPM Redemption*

Location: Dolphin 7

Comment: DAMA had applied a coat to the pile to cap connections.



Subject: Precast Panel

Location: Cruise Dock Trestle

Comment: The crew set Panel PC1 from Grid Line A3 to B3.



Subject: *Ore Loader Platform Demolition.*

Location: *Broadway Yard*

Comment: PPM resumed breaking the Ore Loader Platform.



Subject: *PPM Lifter*

Location: Dolphin 2

Comment: The crew extracted temporary pile 2B.



Subject: *PPM Lifter*

Location: Dolphin 2

Comment: The crew extracted temporary template pile 2C.



Subject: *PPM Redemption*

Location: Dolphin 7

Comment: DAMA had applied a coat to the pile to cap connections.



Subject: *PPM Redemption*

Location: Dolphin 7

Comment: DAMA coated the pile to cap connection as well as repaired damaged coatings in other areas.



Subject: *PPM Redemption*

Location: Dolphin 7

Comment: DAMA had applied a second coat to the pile to cap connections.



Subject: *PPM Redemption*

Location: Dolphin 7

Comment: DAMA had applied a second coat to the pile to cap connections.



Subject: *Ore Loader Platform Demolition.*

Location: *Broadway Yard*

Comment: PPM completed demolishing this section of the Ore Loader Platform.



Subject: *Ore Loader Platform Demolition.*

Location: *Broadway Yard*

Comment: PPM exposed two previously installed grout tubes from the cap to the beams that had been installed approximately 10' apart.



Subject: *Ore Loader Platform Demolition.*

Location: *Broadway Yard*

Comment: PPM had recovered a sample of a grout tube previously installed through the cap into the beam. This tube did not extend into the beam.

*William Hammac*

William Hammac

04.06.2024

Inspector Signature

Name

Date

**Ore Peninsula Redevelopment Project**

**OWNER:** Municipality of Skagway  
**DESC:** PCO 013 - Nav Light Installation

**START DATE:** April 16, 2024  
**FINISH DATE:** April 16, 2024

**COST SUMMARY**

DESCRIPTION OF WORK	COST TYPE	QUANT.	UNITS	UNIT RATE	LABOR (LAB)	SUPPLIES (SUP)	PPM EQUIP (PPM)	RENT EQUIP (RENT)	SUBCONT (SUB)	PERM MAT'L (PM)
<b>Pacific Pile &amp; Marine</b>										
Labor	LAB	1.0	LS	\$461.21	\$461					
Equipment-Owned	PPM	1.0	LS	\$102.79			\$103			
Equipment-Rental	RENT	1.0	LS	\$695.84				\$696		
Materials	PM	1.0	LS	\$14.00						\$14

Costs only for navigation light installation. Catwalk salvage in lieu of demolition at no added cost.

	LABOR	SUPPLIES	PPM EQUIP	RENT EQUIP	SUBS	PERM MATERIAL
<b>SUBTOTALS:</b>	\$461.21	\$0.00	\$102.79	\$695.84	\$0.00	\$14.00
Taxes @: 0.00%						
OH and Fee	15.0%	10.0%	10.0%	10.0%	10.0%	10.0%
Markup Totals	\$69.18	\$0.00	\$10.28	\$69.58	\$0.00	\$1.40
<b>ITEM TOTALS</b>	\$530.39	\$0.00	\$113.07	\$765.42	\$0.00	\$15.40

<b>TOTAL AMOUNT</b>	<b>\$1,424.28</b>
<b>Bond</b>	<b>1.0%</b>
<b>SUBTOTAL</b>	<b>\$1,438.53</b>
<b>GRAND TOTAL</b>	<b>\$1,438.53</b>

**Force Account Worksheet- Salvage Handrail**

**Work Affected:** 4/16/24 - Reinstall navigation light on Dolphin 1

**Date:** 4/16/2024

**FA Submittal No.:** PCO 013 - Nav Light Installation

**LABOR**

EMPLOYEE NAME	CLASS	PER DIEM	HRS	RATE	COST
	OE Superintendent II	\$3.96	0.5	\$111.36	\$59.64
	PD Foreman	\$17.00	1.0	\$88.70	\$105.70
	PD Journeyman Welder	\$34.00	2.0	\$82.02	\$198.04
	PD Journeyman	\$17.00	1.0	\$80.83	\$97.83
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
					\$0.00
<b>TOTALS:</b>		<b>\$0.00</b>	<b>0.0</b>		<b>\$461.21</b>

**Rate Description:**

**Labor Clarifications**  
 Craft Labor classifications per certified payroll.  
 Staff labor classifications are fully burdened.  
 Per diem costs (housing and meals) are prorated per a 12 hour working day based on actual housing costs during time of work.  
 Labor rates are fully burdened costs (Base rate + tax + union contributions + Workers compensation).  
 Straight/Overtime/Doubletime rates included as applicable to the day (weekday/weekend/holiday) and time of day of work performed.

**Equipment Clarifications**  
 Owned equipment rates per Rental Rate Blue Book - Alaska South region adjustment.  
 Markups per 00 72 00 11.4.A. of the Contract.

**\$461.21**

MARKUP @: 15% \$69.18  
 SUBTOTAL: \$530.39

**EQUIPMENT**

EQUIP. DESCRIPTION	WORKING TIME			STANDBY TIME			TOTAL
	HRS	RATE	COST	HRS	RATE	COST	
Welding Machine	2.0	\$7.57	\$15.14			\$0.00	\$15.14
Jilly Basket	2.0	\$0.00	\$0.00			\$0.00	\$0.00
Pacific Lifter (Rental)	0.5	\$1,391.67	\$695.84			\$0.00	\$695.84
16' Work Skiff	0.5	\$20.50	\$10.25				\$10.25
10k Telehandler	0.5	\$154.80	\$77.40				\$77.40
			\$0.00				\$0.00
<b>TOTALS:</b>	<b>5.5</b>		<b>\$798.63</b>	<b>0.0</b>		<b>\$0.00</b>	<b>\$798.63</b>

MARKUP @: 10.0% \$79.86  
 SUBTOTAL: \$878.49

**SUBCONTRACTOR & MATERIAL INVOICING**

NAME	DESCRIPTION	Work Impacted	TOTAL
Welding Consumables	2 HR at \$7/hr		\$14.00
<b>TOTALS:</b>			<b>\$14.00</b>

MARKUP @: 10.0% \$1.40  
 SUBTOTAL: \$15.40



SUMMARY OF COSTS				
Cost		Markup		Totals
LABOR:	\$461.21	+	\$69.18	= \$530.39
EQUIP:	\$798.63	+	\$79.86	= \$878.49
MATERIALS:	\$14.00	+	\$1.40	= \$15.40
<b>TOTALS:</b>	<b>\$1,273.83</b>	<b>+</b>	<b>\$150.44</b>	<b>= \$1,424.28</b>

This T&M sheet represents the direct cost associated with the activities on the date notated at the top of the sheet. Cumulative impact on project schedule and cost is not included within this breakdown and will be evaluated separately.

PPM Representative: \_\_\_\_\_  
 MOS Representative: \_\_\_\_\_



**Ore Peninsula Redevelopment Project**

**OWNER:** Municipality of Skagway  
**DESC:** PCO 034 - CD Trestle Pile Coating

**COST SUMMARY**

DESCRIPTION OF WORK	COST TYPE	QUANT.	UNITS	UNIT RATE	LABOR (LAB)	SUPPLIES (SUP)	PPM EQUIP (PPM)	RENT EQUIP (RENT)	SUBCONT (SUB)	PERM MAT'L (PM)	
<b>DAMA Industrial</b>											
Labor/Equipment (2-man Paint Crew)	SUB	2.0	DAY	\$2,500.00					\$5,000		
Materials (Extra Paint Kits)	PM	5.0	EA	\$200.00						\$1,000	
Lodging (PPM provided)	SUP	2.0	DAY	\$109.00		\$218					
<b>Pacific Pile &amp; Marine</b>											
Coating Material/Equipment Handling	LAB	6.0	HR	\$108.84	\$653						
10K Telehandler	PPM	3.0	HR	154.80			\$464				
<b>SUBTOTALS:</b>					<b>LABOR</b>	<b>SUPPLIES</b>	<b>PPM EQUIP</b>	<b>RENT EQUIP</b>	<b>SUBS</b>	<b>PERM MATERIAL</b>	
					\$653.04	\$218.00	\$464.40	\$0.00	\$5,000.00	\$1,000.00	
Taxes @: 0.00%											
OH and Fee					15.0%	10.0%	10.0%	10.0%	10.0%	10.0%	
Markup Totals					\$97.96	\$21.80	\$46.44	\$0.00	\$500.00	\$100.00	
<b>ITEM TOTALS</b>					<b>\$751.00</b>	<b>\$239.80</b>	<b>\$510.84</b>	<b>\$0.00</b>	<b>\$5,500.00</b>	<b>\$1,100.00</b>	
<b>TOTAL AMOUNT</b>										<b>\$8,101.64</b>	
<b>Bond</b>										<b>1.0%</b>	<b>\$81.02</b>
<b>SUBTOTAL</b>										<b>\$8,182.65</b>	
<b>GRAND TOTAL</b>										<b>\$8,182.65</b>	

**Ore Peninsula Redevelopment Project**

**OWNER:** Municipality of Skagway  
**DESC:** PCO 037 - Access Ramp Transition Plate Testing

**START DATE:** March 25, 2024  
**FINISH DATE:** March 25, 2024

**COST SUMMARY**

DESCRIPTION OF WORK	COST TYPE	QUANT.	UNITS	UNIT RATE	LABOR (LAB)	SUPPLIES (SUP)	PPM EQUIP (PPM)	RENT EQUIP (RENT)	SUBCONT (SUB)	PERM MAT'L (PM)
Jesse Co. - Facilitate Ramp Testing	SUB	1.0	LS	\$3,400.00					\$3,400	
Swivel Hoist Rings	SUP	1.0	LS	\$874.65		\$875				
<b>SUBTOTALS:</b>					\$0.00	\$874.65	\$0.00	\$0.00	\$3,400.00	\$0.00
Taxes @: 0.00%										
OH and Fee					15.0%	10.0%	10.0%	10.0%	10.0%	10.0%
Markup Totals					\$0.00	\$87.47	\$0.00	\$0.00	\$340.00	\$0.00
<b>ITEM TOTALS</b>					\$0.00	\$962.12	\$0.00	\$0.00	\$3,740.00	\$0.00

<b>TOTAL AMOUNT</b>	\$4,702.12
<b>Bond</b>	1.0% \$47.02
<b>SUBTOTAL</b>	<b>\$4,749.14</b>
<b>GRAND TOTAL</b>	<b>\$4,749.14</b>





FABRICATION MACHINERY EQUIPMENT

1840 MARINE VIEW DRIVE, TACOMA, WA 98422

O: (253) 922-7433 / F: (253) 922-1998 / JESSE-CO.COM

**BUILDING A LEGACY 40 YEARS STRONG**

Page: 1  
 Invoice Date: 03/29/2024  
 Terms: Net 30 days  
 Due Date: 04/28/2024

Customer P.O. #	Cust. I.D. #	Job #	Shipped by	Proj. Mgr.
23009	PACPILE	02093		DAVID EASTERLY

Sold To: PACIFIC PILE & MARINE  
 ap@pacificpile.com  
 700 SOUTH RIVERSIDE DRIVE  
 SEATTLE WA 98108

Ship To: BOYER FACILITY  
 7318 4TH AVE SO  
 SEATTLE WA 98108

Item/Description	Units	Price Each	Extended Cost
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]	[REDACTED]	[REDACTED]
7 CO #06 - TECH SUPPORT-RAMP TEST, GUARD MODS	1.00	3,400.00	3,400.00

Please do not accept any requests for a change in payment method via email or letter. Please verify any requests verbally by calling (253) 922 7433 and asking for Accounts Receivable

**TOTAL LINE ITEMS** [REDACTED]

**TOTAL INVOICE US \$** [REDACTED]

# West Coast Wire Rope & Rigging Inc.

# INVOICE

Branch: 20

2900 NW 29th Avenue  
 PORTLAND, OR 97210  
 USA

PHONE: 503-228-9353  
 FAX: 503-228-2435  
 EMAIL: AR@WCWR.COM

INVOICE	
5623397	
Invoice Date	Page
03/25/2024 08:37:02	1 of 1
ORDER NUMBER	
1759901	

**\*\*DIRECT SHIPMENT\*\***

**PACIFIC PILE & MARINE LP [E]**  
 700 S RIVERSIDE DR  
 SEATTLE, WA 98108

**Ship To:**

Jesse Co C/O PACIFIC PILE & MARINE LP [E]  
 Skagway Ore Peninsula Redevelopment Attn: Ale  
 1840 Marine View Dr  
 TACOMA, WA 98422

Customer ID: 17088

Ordered By: Shane Hautanen

Term Description	Net Due Date	Disc Due Date	Discount Amount	Resale Certificate
NET 30	04/24/2024	04/24/2024	0.00	NONE

Order Date	Pick Ticket No	PO Number	Sales Rep
03/22/2024 13:36:30	3628750	32307	ASHLEIGH

Quantities					Item ID Item Description	Pricing UOM Unit Size	Unit Price	Extended Price
Ordered	Shipped	Remaining	UOM Unit Size	Disp.				

**Delivery Instructions:** Please deliver by 0800 PST Monday 3/25

**Carrier:** UPS - RED NEXT DAY AIR

**Tracking #:** 1ZA4776R1571210412

4.000	4.000	0.000	EA		46006NS	EA	131.8000	527.20
			1.0		5/8-11 X E= 1-1/4" SWIVEL HOIST RING	1.0		

Total Lines: 1		<b>SUB-TOTAL:</b>	527.20
Total Freight In: 0.00	Total Freight Out: 266.13	<b>TOTAL FREIGHT:</b>	266.13
		<b>TAX:</b>	81.32
		<b>AMOUNT DUE:</b>	<b>874.65</b>

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